

High Speed Rail (Crewe – Manchester)

Supplementary Environmental Statement 1 and Additional Provision 1 Environmental Statement

Volume 5: Appendix LV-001-0MA02

Landscape and visual

Landscape and visual impact assessment and photomontages

MA02: Wimboldsley to Lostock Gralam

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MA02: Wimboldsley to Lostock Gralam



Department
for Transport

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Part 1: Introduction

1.1 Structure of this appendix

- 1.1.1 This document is an appendix to the landscape and visual impact assessment (LVIA) which forms part of Volume 5 of the Supplementary Environmental Statement 1 (SES1) and Additional Provision 1 Environmental Statement (AP1 ES) for the Wimboldsley to Lostock Gralam community area (MA02).
- 1.1.2 This appendix provides details of changes to the LVIA since the production of the High Speed Two (HS2) High Speed Rail (Crewe – Manchester) Environmental Statement (ES)¹ published in 2022 (the main ES).
- 1.1.3 This document comprises of four parts:
- Part 1: introduction;
 - Part 2: a summary of engagement with technical stakeholders;
 - Part 3: the Supplementary Environmental Statement 1; and
 - Part 4: the Additional Provision 1 Environmental Statement.
- 1.1.4 This report should be read in conjunction with the main ES Volume 5, Appendix: LV-001-0MA02.
- 1.1.5 Maps referred to in this appendix are contained in the SES1 and AP1 ES Volume 5, Landscape and visual Map Book.
- 1.1.6 In order to differentiate between the original proposals assessed as part of the main ES and subsequent changes, the following terms are used:
- ‘the original scheme’ – the Bill scheme submitted to Parliament in January 2022, which was assessed in the main ES;
 - ‘the SES1 scheme’ – the original scheme with the changes described in SES1 that are within the existing powers of the Bill; and
 - ‘the AP1 revised scheme’ – the original scheme as amended by the SES1 changes and AP1 amendments.

¹ High Speed Two Ltd (2022), High Speed Rail (Crewe – Manchester), *Environmental Statement*. Available online at: www.gov.uk/government/collections/hs2-phase2b-crewe-manchester-environmental-statement.

1.2 Landscape character assessment

1.2.1 This section describes the landscape baseline and assesses the effects on landscape, with reference to the landscape character areas (LCA) defined for the Wimboldsley to Lostock Gralam area (MA02). A summary of the landscape baseline and significant landscape effects is provided in the SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02). The SES1 and AP1 ES Volume 5 Map Book: Map Series LV-02, should also be read in conjunction with this section. Elements of landscape are shown in the diagram on the right.

1.2.2 This section is organised as follows:

- information on each LCA within the area including a description of the landscape (with Ordnance Survey (OS) maps and photography to help illustrate character and patterns of land uses and vegetation of the area), as well as an analysis of the value, susceptibility and sensitivity of each LCA. These are ordered from south to north along the route of the AP1 revised scheme;
- description of future baseline conditions, where relevant; and
- assessment of the effects of the SES1 scheme and/or AP1 revised scheme on the landscape at construction, and at year 1, year 15 and year 30 of operation.

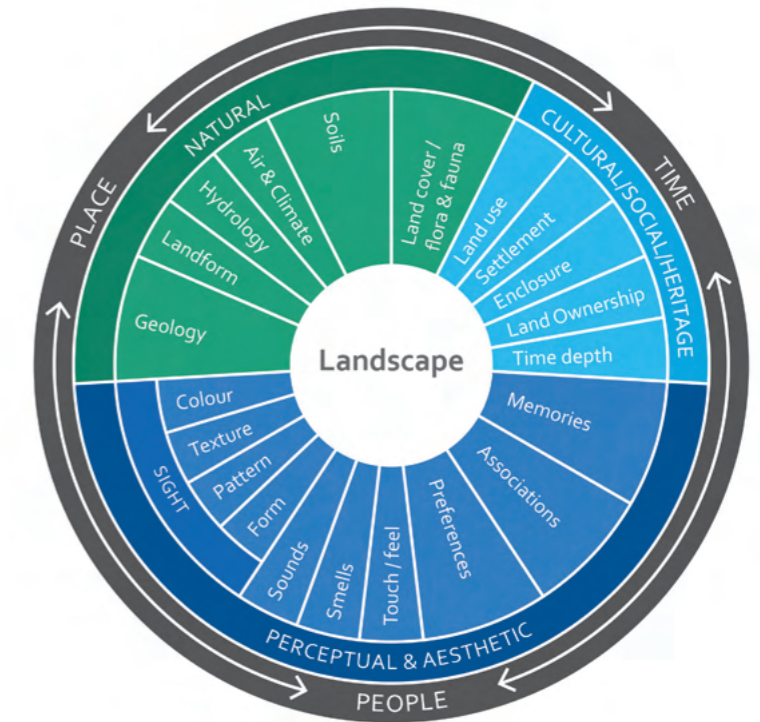
1.3 Approach to landscape characterisation

1.3.1 The LCA have been determined as part of an integrated approach to environmental characterisation in collaboration with other environmental topics including historic environment and ecology and biodiversity, with reference to a number of published studies at the national, county and district level. A wide variety of spatially referenced data were also reviewed in developing the landscape characterisation, including existing landscape/townscape characterisations, historic landscape characterisation (HLC), Phase 1 Habitat Survey, geological and hydrological data and aerial photography. Such data have also been used, along with field survey, to consider sub-divisions to existing published LCA, where appropriate. These sub-divisions have been made on the basis of scale and for appropriate recording of specific landscape variations and susceptibilities to change resulting from the SES1 scheme and/or AP1 revised scheme.

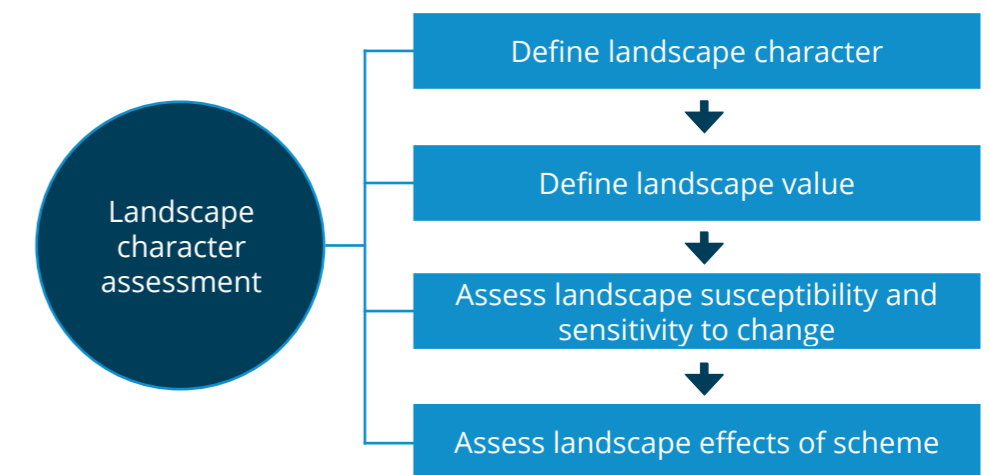
1.3.2 The national landscape character context is illustrated on SES1 and AP1 ES Volume 5, Landscape and visual Map Book: Map Series LV-00. The county/district landscape character types (LCT) of relevance to the Wimboldsley to Lostock Gralam area are summarised below:

- settled heathlands: A gently rolling lowland landscape of acid sandy soils now mainly overlaid with mixed farming in a regular rectilinear field pattern, with many well-wooded areas;
- terrace alluvial lowlands: A landscape type associated with older river terraces, relatively remote from the floodplain. A flat landscape of predominantly intensive arable farmland and improved pasture, with pockets of ancient, irregular fields;
- riparian alluvial lowlands: A landscape of levels and lowland river valleys, where alluvial soils, and occasionally peat, overlay alluvial drift deposits. Pasture is the dominant land use, with waterside trees such as willow and alder predominating. Valley parklands are also apparent;
- settled farmlands: Landscapes of undulating lowlands and hills, thinly scattered with woodlands of often ancient origin and a mixed, though not distinctive settlement pattern; and
- settled plateau farmland slopes: An undulating landscape shaped by glacial drift deposits, supporting dairying and mixed farming in a semi regular hedged field pattern with some woodlands, often of ancient origin, and remnant heathlands.

1.3.3 Descriptions of the LCA affected by the SES1 scheme and/or AP1 revised scheme identified in the Wimboldsley to Lostock Gralam area are provided in Part 3 and Part 4. The LCA are shown on the SES1 and AP1 ES Landscape and visual Map Book: Map Series LV-02. A summary description of the LCA most likely to be affected is included in the SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02). The routewide landscape context is presented on SES1 and AP1 ES Landscape and visual Map Book: Map Series LV-17, providing an overview of the SES1 scheme/AP1 revised scheme in the context of the LCA.



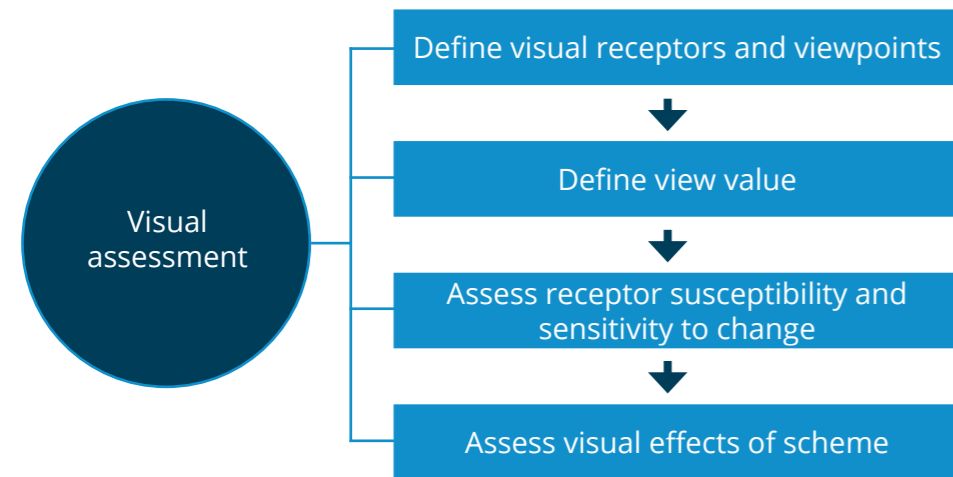
Above: The elements of landscape. Diagram is based on 'An Approach to Landscape Character Assessment' Natural England, 2014



Above: The landscape assessment process

1.4 Visual assessment

- 1.4.1 Descriptions of the identified viewpoints are provided in this section. The viewpoints are shown on SES1 and AP1 ES Volume 5, Landscape and visual Map Book: Map Series LV-07 (construction) and Map Series LV-08 (operation). For each viewpoint, the first part of the baseline description relates to the view during the winter and the second part relates to the summer view. Where relevant the third part relates to the view at night time and the fourth part to the future baseline.
- 1.4.2 The assessment considers the value of the view and the susceptibility of the viewer to the SES1 scheme and/or AP1 revised scheme, and the overall sensitivity of the visual receptors.

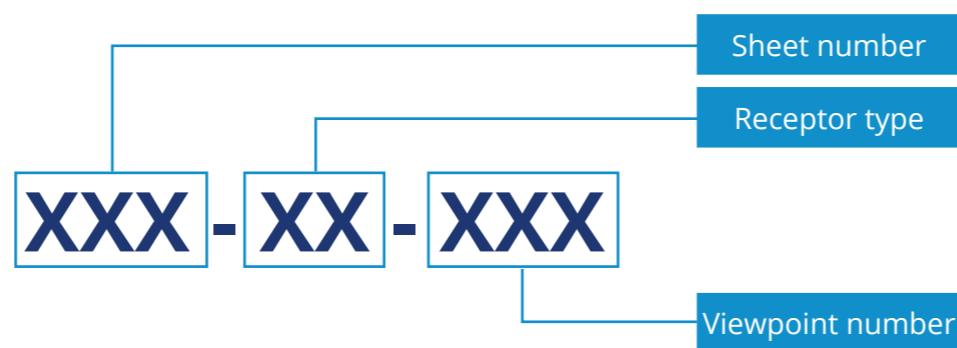


Above: The visual assessment process

- 1.4.3 Effects have been assessed where relevant for construction, operation year 1, year 15 and year 30. A summary of all significant visual effects is given in the SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02), Sections 3 and 4.
- 1.4.4 Photographs have been included to represent the view from visual receptors during winter and, where relevant, summer. For some visual receptors no appropriate or accessible location from which to capture representative photographs of the view was available, therefore no photograph has been included and the assessment has been undertaken based on professional judgement.
- 1.4.5 All photography included within this document has been taken in accordance with the methodology set out within the Technical Note - Approach to photography contained within the Environmental Impact Assessment Scope and Methodology Report (SMR) (Volume 5, Appendix: CT-001-00001)² of the main ES.
- 1.4.6 Photomontages have also been included for relevant viewpoints. All photography associated with photomontages is verifiable and has been taken in accordance with the Technical Note - Approach to verifiable photomontages, contained within the SMR (main ES Volume 5, Appendix: CT-001-00001). All verifiable photography includes additional image specification and data information.

1.5 Visual receptors

- 1.5.1 The number on each viewpoint identifies the viewpoint locations which are shown on SES1 and AP1 ES Volume 5, Landscape and visual Map Book: Map Series LV-07 (construction) and Map Series LV-08 (operation). The following numbering convention is used:



Above: Viewpoint numbering convention

- 1.5.2 In each case, the middle number (xxx.xx.xxx) identifies the type of receptor represented, as described below (with more detail in the SMR of the main ES):
01. Protected views – these relate to those viewpoints, panoramas and viewing corridors that have been designated by local authorities, county councils or other relevant stakeholders. People enjoying protected views have a high susceptibility to change;
02. Residential views – residents have a high susceptibility to changes in their views, as attention is often focused on the landscape surrounding the property, rather than on another focused activity (as will be the case in predominantly employment or industrial areas);
03. Recreational views – these receptors generally have a high susceptibility to changes in their views, as attention is focused on the enjoyment of the landscape. Receptors engaged in activities whereby attention is focused on the surrounding landscape also have a high susceptibility to changes in their views;
04. Transport views – travel through an area is often the means by which the greatest number of people view the landscape. Because of the glimpsed nature of the view from trains or vehicles, people traveling through an area on main roads have a low susceptibility to changes in their views, while those on scenic routes have a medium susceptibility. People travelling through urban areas in vehicles have a low susceptibility to changes in their views although in residential areas this increases to medium;
05. Hotels and healthcare institutions – people staying in hotels or healthcare institutions and schools have periods of time where their attention may be focused on the landscape, whilst at other times attention is more likely to be focused on other activities. Based on the level of interaction with the surrounding landscape, these receptors have a medium susceptibility to changes in their views; and
06. Employment – people at work and within educational institutions (other than residential educational facilities) are the least susceptible receptors, as their attention is likely to be focused on their work activity. These receptors have a low susceptibility to changes in their views.
- 1.5.3 Night time visual survey and assessments have only been undertaken where continuous working during construction or additional lighting in operation has the potential to result in significant effects on residential and certain recreational receptors. Further detail is set out within the Technical Note - Approach to night time assessment, contained within the SMR of the main ES.

² High Speed Two Ltd (2022), High Speed Rail (Crewe – Manchester), Environmental Statement, Environmental Impact Assessment Scope and Methodology Report, Volume 5, Appendix: CT-001-00001. Available online at: <https://www.gov.uk/government/collections/hs2-phase2b-crewe-manchester-environmental-statement>.

Part 2: Engagement with technical stakeholders

2.1 Introduction

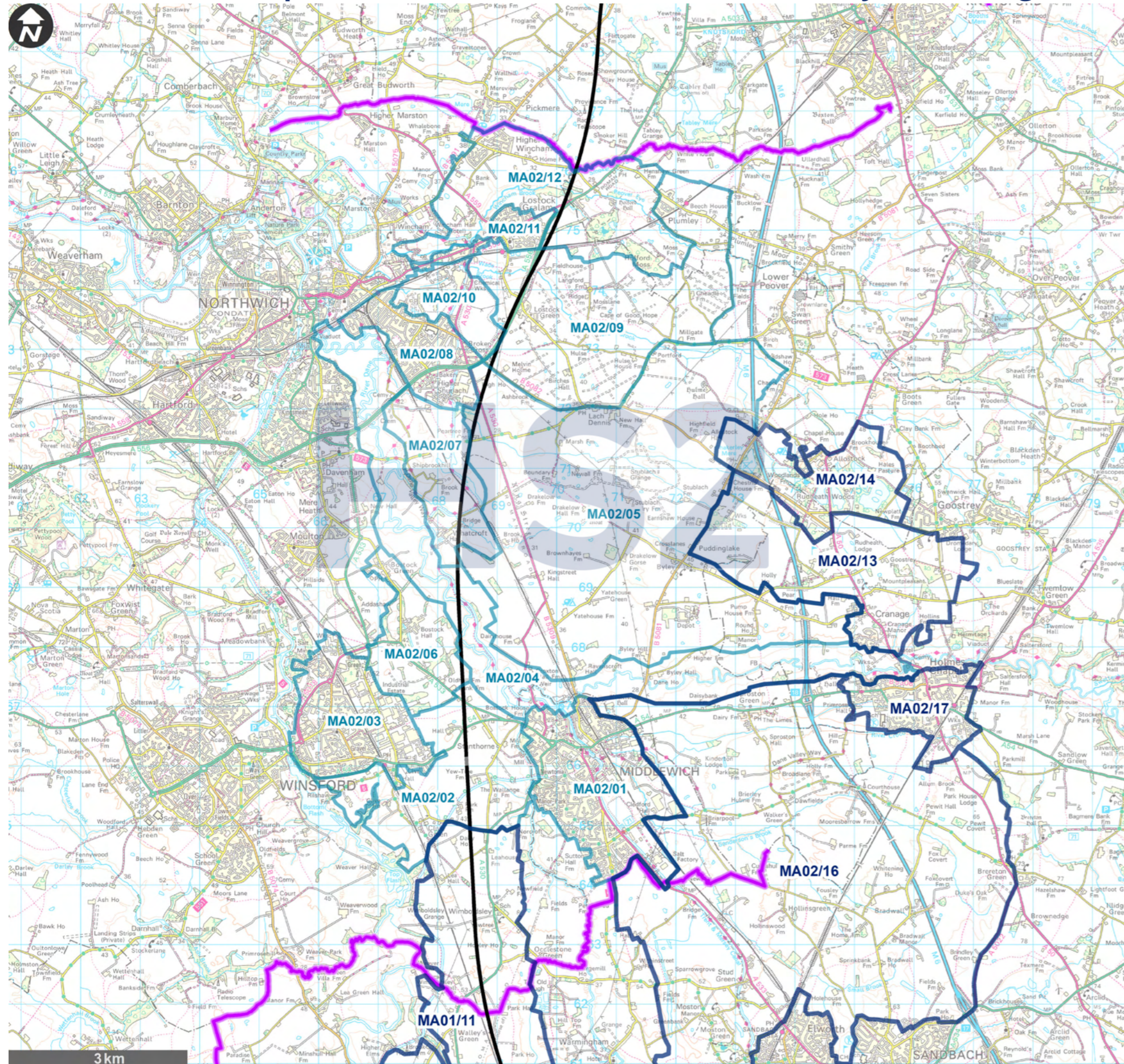
- 2.1.1 It has not been possible to discuss relevant LCA and viewpoints with local authorities. However, there will be further engagement with local authorities regarding this matter during the passage of the Bill.

Part 3: Supplementary Environmental Statement 1

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3.1 Landscape character assessment

Overview of landscape character areas within MA02 affected by SES1 changes



Landscape character areas assessed as part of SES and in the main ES

- MA01/11 - Wimboldsley Plain
- MA02/13 - Cranage and Byley Sandy Woods
- MA02/14 - Rudheath Woods, Shakerley and New Platt Meres Sandy Woods

Landscape character areas assessed as part of AP1 only

- MA01/11 - Wimboldsley Plain
- MA02/16 - Cheshire East/Wimboldsley Plain
- MA02/17 - Holmes Chapel

Landscape character areas assessed in the main ES only

- MA02/01 - Middlewich
- MA02/02 - Winsford and Middlewich Fringe Farmland
- MA02/03 - Winsford East
- MA02/04 - Dane Valley
- MA02/05 - Stublach Plain
- MA02/06 - Bostock Hall Estate
- MA02/07 - Whatcroft and Billinge Green Flashes
- MA02/08 - Rudheath
- MA02/09 - Lostock Plain
- MA02/10 - Rudheath Lime Beds
- MA02/11 - Lostock Gralam
- MA02/12 - Wincham Brook and Holford Lower Wooded Farmland

— Route in tunnel — Community area boundary
 — Route on surface

Cranage and Byley Sandy Woods LCA

Landscape character baseline description

This is a well wooded area associated with former grazed heathland and still retains a heathy character. It is defined by blocks of mixed woodland and predominantly arable farmland interspersed with small relict heath and mosses.

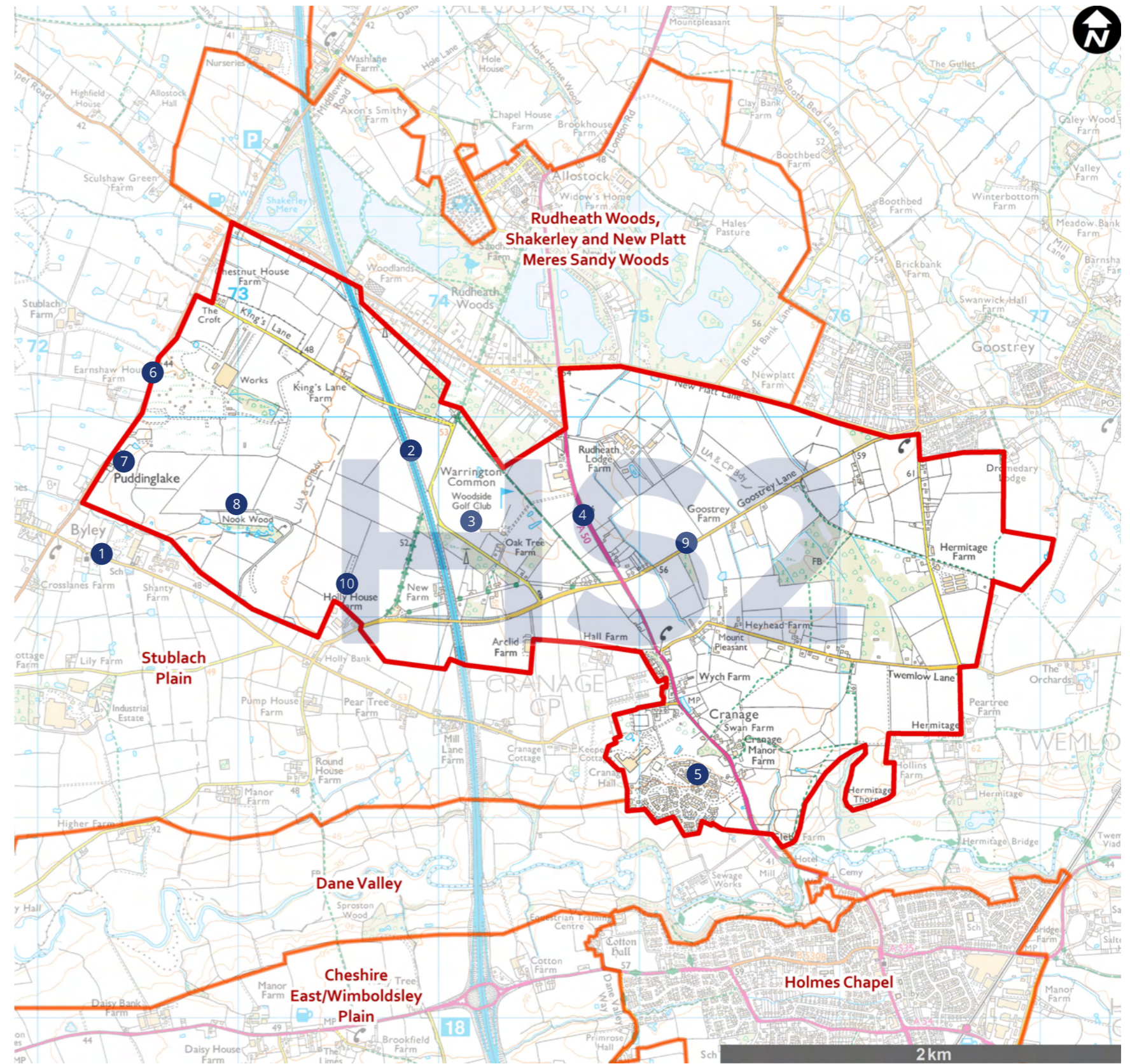
The field pattern is of a medium to large-scale with a mix of arable and pasture, crossed by brooks. Numerous small ponds and mosses formed in glacial hollows are now often hidden within dense woodland. Beyond the woodlands, the flat or undulating landscape consists of large fields defined by straight hedgerow boundaries. Many of the fields are defined by linear blocks of trees, including conifer plantations, which dominate the skyline and generally restrict views out of the area.

A number of substantial properties and grazing enclosures occupy plots which appear to have been cut out of the dense woodland. The only vantage points are provided by the bridges constructed over the busy M6 motorway as it runs north-south through the area. This major highway is not as visually intrusive as one might expect, due to the presence of mature roadside planting, the visual constraint associated with woodland cover and the lack of high viewpoints. However, there is loss of tranquillity due to the presence of traffic movements.



Key landscape characteristics

A generally well-managed flat to gently undulating landscape with dispersed settlement and a visual contrast between the enclosed nature of the woodland and the openness of the large arable fields. Some sense of rural tranquillity away from the major road networks to the east of the LCA.



- Route on surface
- LCA boundary
- 1 Byley
- 2 M6
- 3 Woodside Golf Club
- 4 A50 Knutsford Road
- 5 Cranage
- 6 B5081 Middlewich Road
- 7 Puddinglake
- 8 Nook Wood
- 9 Goostrey Lane
- 10 Holly House Farm

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Key landscape value attributes

The key landscape value attributes of the Cranage and Byley Sandy Woods LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

This is a landscape of strong contrasts where the flat or gently rolling topography and large straight-sided fields combine with woodland blocks to create a large-scale landscape. The mosaic of woodland and relict heathland are both a legacy of glaciation and former sand and gravel quarries which provide some sense of naturalness and visual diversity (a variety of colours and textures and changing seasonal interest) and a setting for recreation and enjoyment of the landscape. Views over the large arable fields to the Pennine Hills enable an appreciation of the wider landscape to the east of the LCA. To the west of the LCA, a gas station, large factory works, and road infrastructure are detracting elements within the local landscape.



Cultural, social and historic

The landscape is one of former open heathland, subject to field enclosure in recent centuries for agricultural use. Active quarries continue a longstanding exploitation of the area for the abstraction of sand and gravels, although these tend to be well screened within the LCA by mature tree belts. Development is sparse, with older farmsteads providing a cultural record of the past and reflecting the historic associations of the area with the industrially generated wealth of adjacent towns and cities. The Public Right of Way (PRoW) network is concentrated mainly to the east of the LCA.

Key landscape characteristics susceptible to the SES1 scheme



Overall landscape value

This is a generally flat open landscape with landform, soils, vegetation having been heavily influenced by glacial activity, with numerous meltwater channels and hollows in which ponds and mosses have subsequently developed to the east of the LCA. Blocks of mixed woodland, planted on former sandy heathland, lend a wooded appearance. Large arable fields are defined by straight hedgerows or post and wire fences with some smaller pastoral fields as well as remnant areas of 18th or 19th Century field enclosure from waste heathland. Away from the busy M6 motorway which runs north-south along the Cheshire West border, and key arterial road of the A50, areas of tranquillity can be found in the east of the LCA. To the west of the LCA the gas station and factory works are well screened and integrated adjacent to the B5081 and King's Lane. PRoW, particularly in the east of the LCA provide a valuable recreational resource for cycling, walking and horse riding. The M6 and A50 are generally well integrated into the landscape. The value of this LCA is therefore **medium**.

Overall landscape susceptibility

There are few distinctive or prominent landscape features which are susceptible to the SES1 scheme. Woodland cover and hedgerow connectivity are susceptible as well as any further fragmentation of mosses and heathland. The landscape therefore has a **medium-low** susceptibility to change resulting from the SES1 scheme.

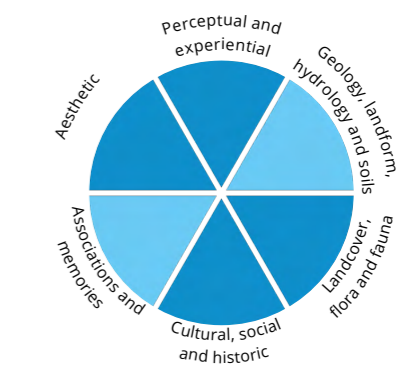
Future baseline

Committed development MA02/252, comprising land at Rudheath Lodge, New Platt Lane, Cranage and Allstock would be utilised for silica sand extraction and associated development, mineral extraction by dredging, progressive restoration, mineral processing and dispatch. The site is likely to be active at the point of construction.

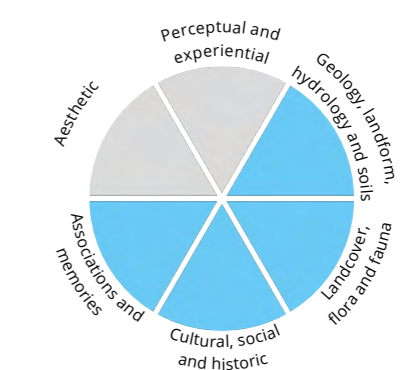
Overall landscape sensitivity

This is a rural landscape which, whilst largely intact with sparse development, contains occasional detracting features including the M6, A50 trunk road, pylons, gas station and large factory works. The landscape has local recreational value, including a golf course, enhanced by proximity to Holmes Chapel and Goostrey and a PRoW network predominantly in the east of the LCA. The sensitivity of this LCA is **medium-low**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

The main ES reported a **low** magnitude of change and a **minor** adverse (non-significant) effect. This would be due to a small proportion of the LCA being affected by the original scheme and construction activity being located in proximity to existing industrial and infrastructure elements. The design change, removal of MA02 Borrow Pit D, north of Moss Lane (SES1-002-002), will remove construction effects associated with Borrow Pit D from the SES1 scheme. The SES1 design change will result in no landscape effects for the Cranage and Byley Woods LCA.

Effects will be **negligible (non-significant)**.

Operation year 1

The main ES reported a **low** magnitude of change and a **minor** adverse (non-significant) effect. This would be due to changes to the local field pattern and the loss of woodland and hedgerows removed during construction. The SES1 design change, removal of MA02 Borrow Pit D (SES1-002-002), will remove operational effects associated with Borrow Pit D from the SES1 scheme. The SES1 design change will result in no landscape effects for the Cranage and Byley Woods LCA.

Effects will be **negligible (non-significant)**.

Operation year 15

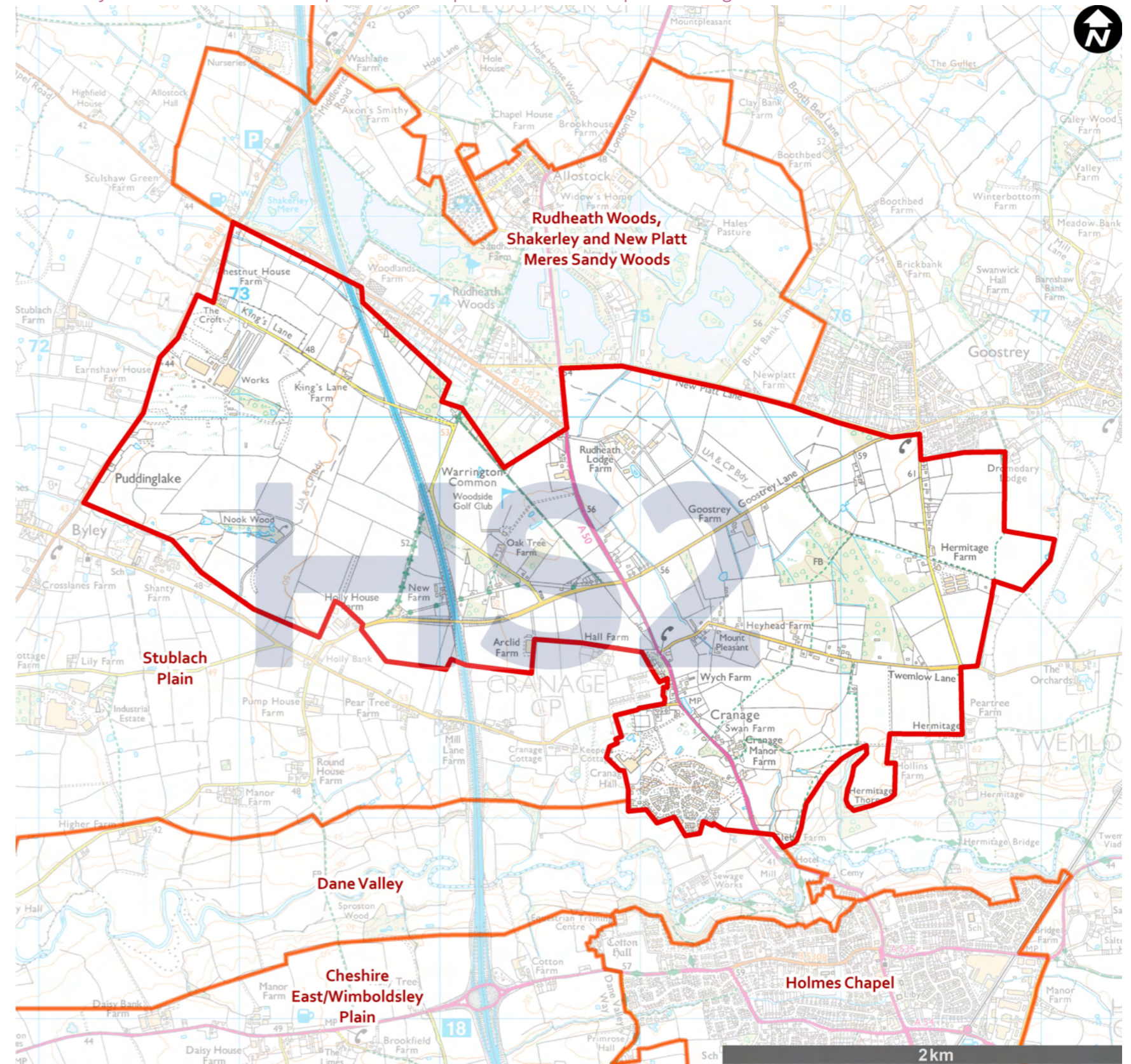
Due to the removal of MA02 Borrow Pit D (SES1-002-002) from the SES1 scheme, there will be no change to the LCA and the negligible (non-significant) effect reported in the main ES will be removed.

Effects will be **negligible (non-significant)**.

Operation year 30

Due to the removal of MA02 Borrow Pit D (SES1-002-002) from the SES1 scheme, there will be no change to the LCA and the negligible (non-significant) effect reported in the main ES will be removed.

Effects will be **negligible (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

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Rudheath Woods, Shakerley and New Platt Meres Sandy Woods LCA

Landscape character baseline description

An open large-scale area of glacio-fluvial deposits on the eastern boundary of the Cheshire West borough with large water bodies and large-scale arable fields.

This is a flat landscape of woodland, farmland and heathland overlying sand and gravel glacio-fluvial drift material, deposited during the last glaciation, lying between 45m and 50m Above Ordnance Datum (AOD).

Areas of plantation woodland on former heath and peatland, including conifers which form a dark skyline. Sandy soils support species rich acid grassland, oak-birch woodland and lowland heath. Open water, woodland and heathland are present and are the result of sand and gravel extraction on former heathland. Shakerley Mere Country Park, Boundary Water Park, Rudheath Woods and PRoW provide good opportunities for recreation.

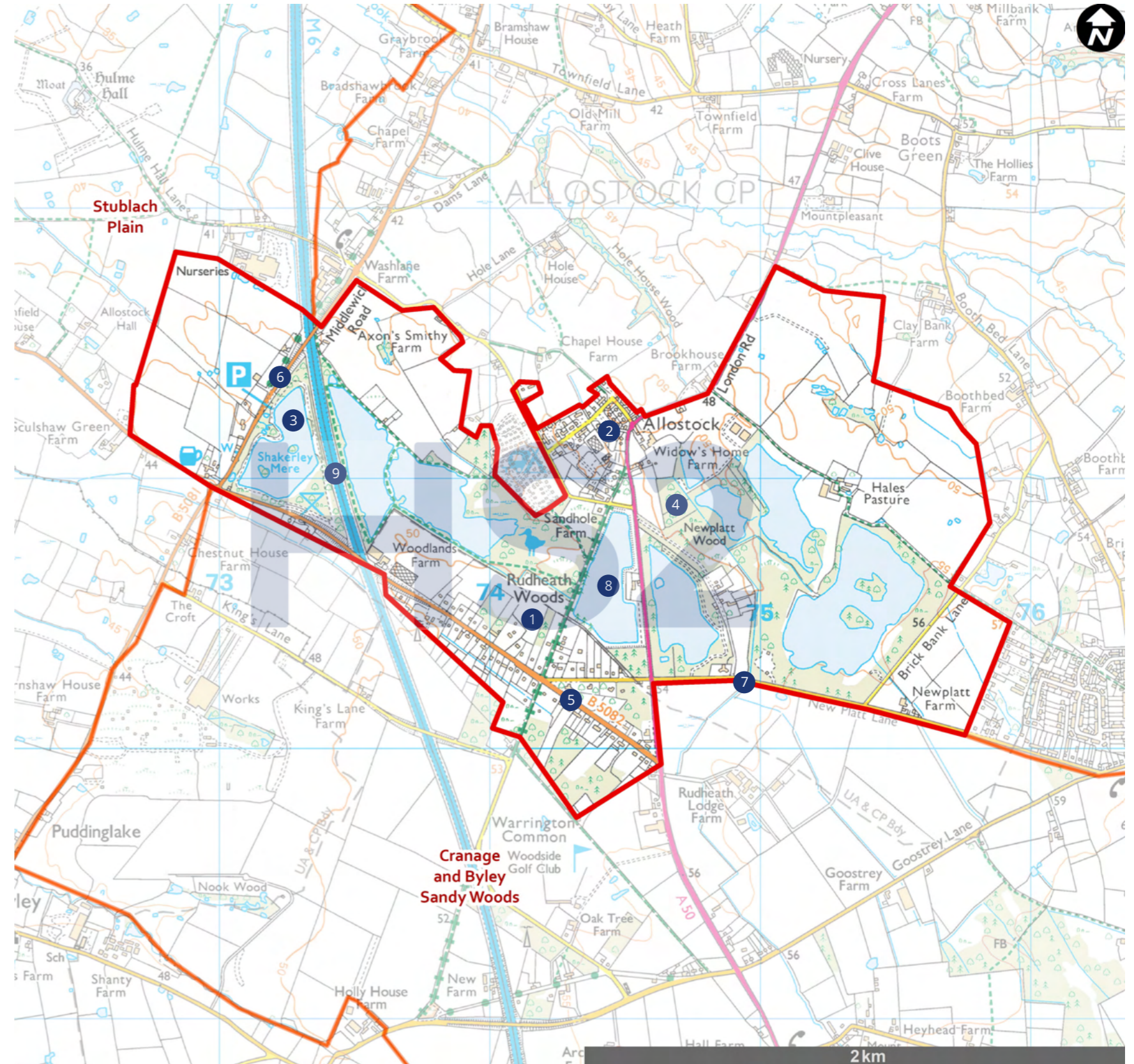
Settlement comprises farmsteads of 18th-19th Century origin set within areas of relatively recent enclosure together with scattered ribbon development alongside roads and a village at Allostock. There is a limited road network and straight roads are bordered by verges and punctuated by gorse which reflects the sandy character of the area.

The general perception is of one of a large-scale landscape due to the flat landform, large-scale fields and large water bodies, which dominate the local landscape character. The scale of the landscape is perceived in open vistas across the meres from close to the water's edge. Elsewhere, dense woodland cover creates a strong sense of enclosure, with close, tree dominated horizons. Low density settlement and dense woodland cover, result in a perception of tranquillity. The M6 divides the east and west sides of the LCA but is well integrated into the landscape by roadside vegetation. Traffic movements along the M6 lower the perception of tranquillity locally.



Key landscape characteristics

A generally flat and large-scale landscape with Shakerley Mere Country Park, Rudheath Wood, Boundary Water Park and PRoW providing recreational opportunities. Open views across the meres, typically surrounded by woodland, are of particular significance and tranquillity is high, away from the M6 corridor.



- Route on surface
- LCA boundary
- 1 Rudheath Woods
- 2 Allostock
- 3 Shakerley Mere
- 4 Newplatt Wood
- 5 B5082 Northwich Road
- 6 B5081 Middlewich Road
- 7 New Platt Lane
- 8 Boundary Water Park
- 9 M6

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Key landscape value attributes

The key landscape value attributes of the Rudheath Woods, Shakerley and New Platt Meres Sandy Woods LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

This is a landscape of strong contrasts. The flat or gently rolling topography and large, regular fields combine with extensive woodland blocks and large water bodies to create a large-scale landscape pattern. In marked contrast, within the woodlands there is a strong sense of enclosure. The woodland is mainly coniferous, with surrounding remnant heathlands. Buildings are characteristically red brick with clay or slate roof tiles, punctuated by whitewashed buildings. The man-made lakes appear natural and contribute to a sense of tranquillity associated with this landscape.



Cultural, social and historic

The legacy of sand and gravel extraction has resulted in a large-scale landscape of large, open waterbodies. There are good opportunities for land and water-based recreation in association with woods, meres and the PRoW network.

Key landscape characteristics susceptible to the SES1 scheme



Overall landscape value

Areas of open water, combined with dense woodland and a good PRoW network, add to the recreational value of this LCA. Enclosing woodland and low density of settlement, result in **high** levels of tranquillity, despite the M6 road corridor cutting through the LCA in a north-south direction. The value of this LCA is therefore **medium-high**.

Overall landscape susceptibility

The dense woodland cover, open views across meres, and the connectivity of recreational assets, are susceptible to changes resulting from the SES1 scheme. Susceptibility is locally lowered by the presence of the M6 corridor. The landscape therefore has a **medium** susceptibility to change resulting from the SES1 scheme.

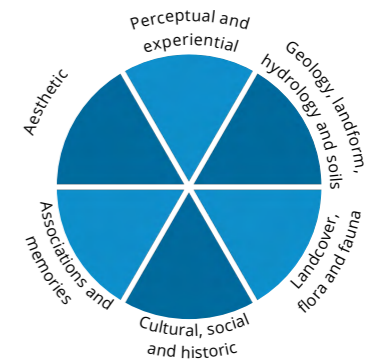
Future baseline

There are no committed developments that will affect the landscape susceptibility to the SES1 scheme.

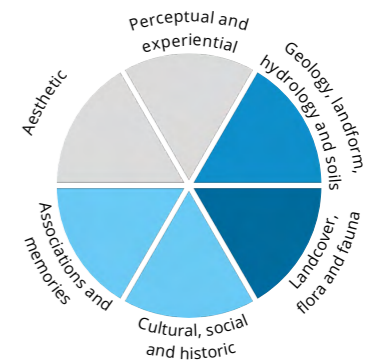
Overall landscape sensitivity

The LCA has high recreational and land cover values combined with a perception of tranquillity across much of the LCA. Susceptibility to change is lowered by the presence of large-scale infrastructure. The sensitivity of this LCA is **medium**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

The main ES reported a **negligible** (non-significant) effect. This would be due to MA02 Borrow Pit D being located in the adjacent Cranage and Sandy Woods LCA, resulting in no direct landscape effects for this LCA. The design change, removal of MA02 Borrow Pit D, north of Moss Lane (SES1-002-002), will remove MA02 Borrow Pit D from the SES1 scheme. The SES1 design change will therefore remove landscape effects for the Rudheath Woods, Shakerley and New Platt Meres Sandy Woods LCA.

Effects will be **negligible (non-significant)**.

Operation year 1

The main ES reported a **negligible** (non-significant) effect. This would be due to the reinstated MA02 Borrow Pit D being located in the adjacent Cranage and Sandy Woods LCA, resulting in no direct landscape effects for this LCA. At year 1, the SES1 design change, removal of MA02 Borrow Pit D (SES1-002-002) will remove effects associated with Borrow Pit D from the SES1 scheme. The SES1 design change will therefore remove landscape effects for the Rudheath Woods, Shakerley and New Platt Meres Sandy Woods LCA.

Effects will be **negligible (non-significant)**.

Operation year 15

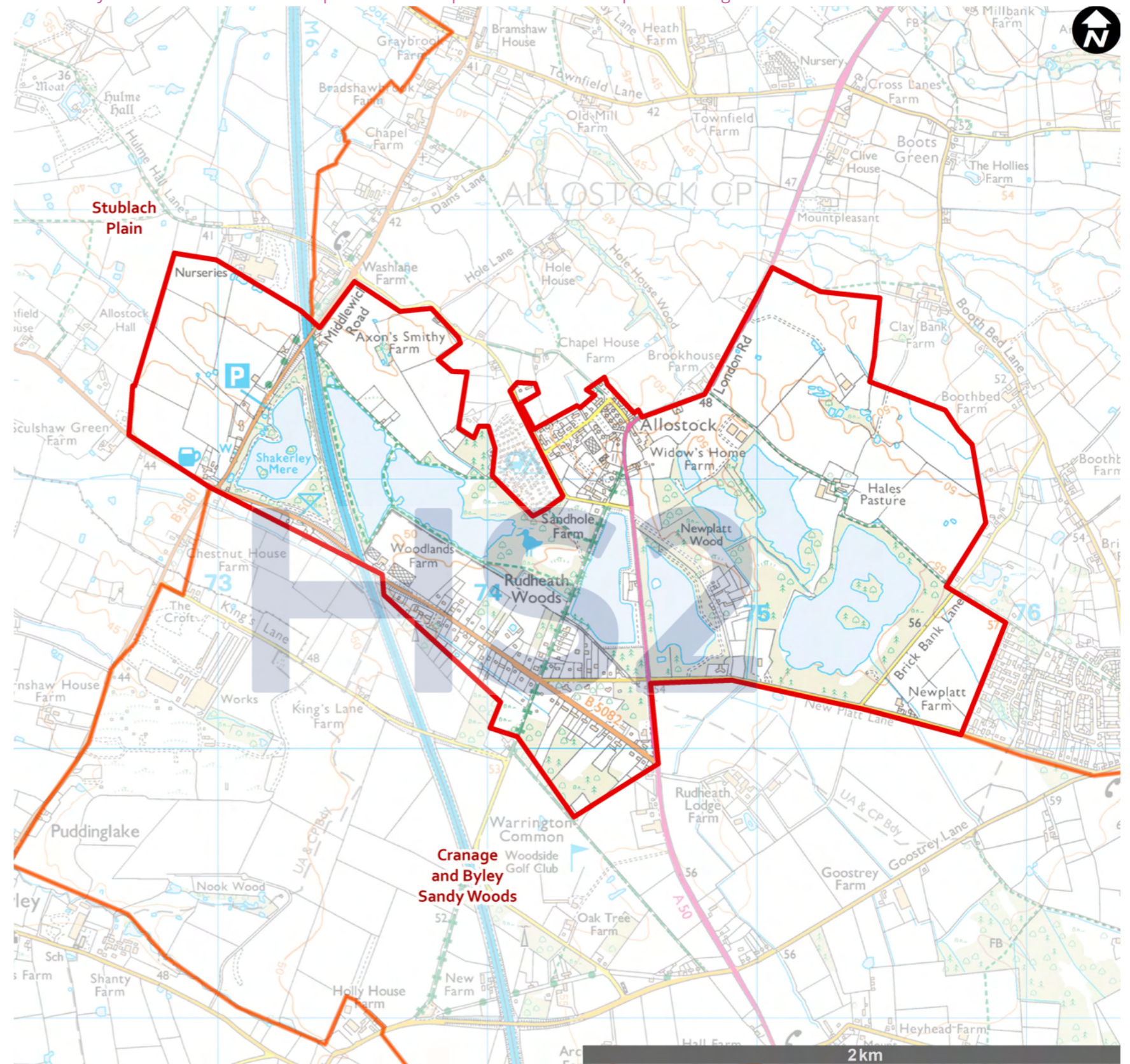
Due to the removal of MA02 Borrow Pit D (SES1-002-002) from the SES1 scheme, there will be no change to the LCA and the negligible (non-significant) effect reported in the main ES will be removed.

Effects will be **negligible (non-significant)**.

Operation year 30

Due to the removal of MA02 Borrow Pit D (SES1-002-002) from the SES1 scheme, there will be no change to the LCA and the negligible (non-significant) effect reported in the main ES will be removed.

Effects will be **negligible (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

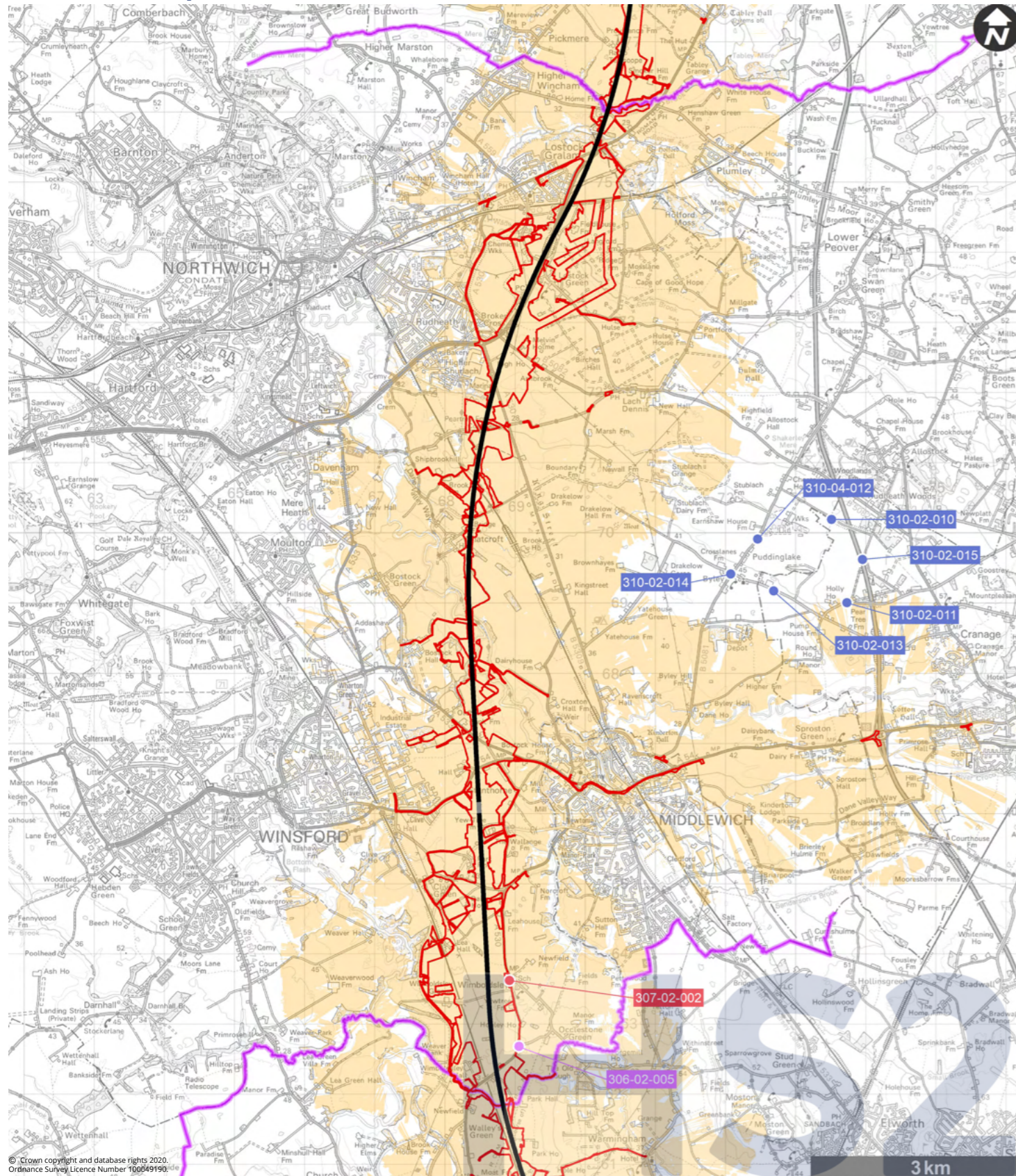
No cumulative effects during construction and no cumulative effect during operation.

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3.2 Visual assessment

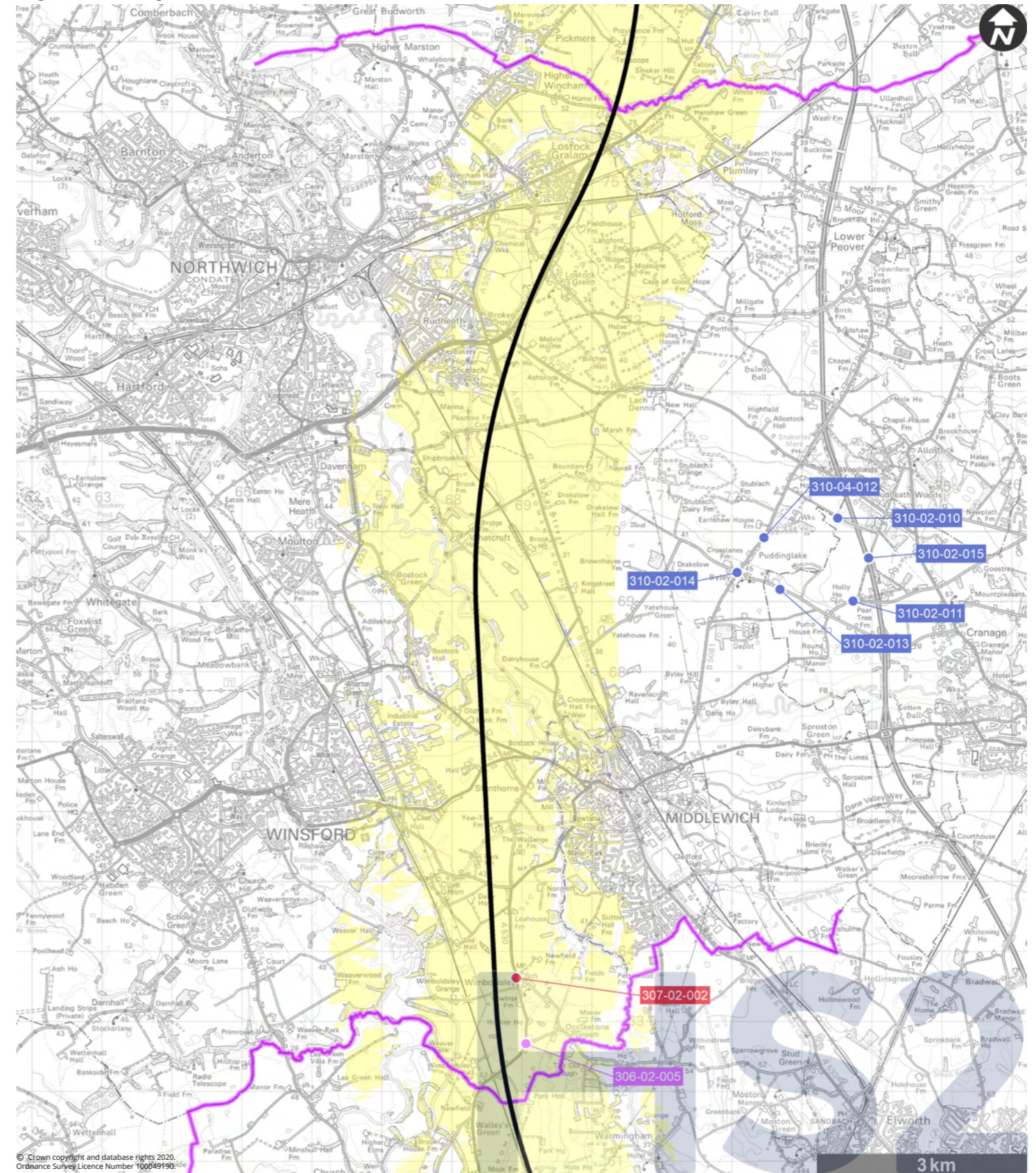
Overview of viewpoints and photomontages in the community area affected by SES1 changes

Construction phase



- Route in tunnel
- Route on surface
- Community area boundary
- Non significantly affected viewpoint
- Significantly affected viewpoint
- Non significantly affected photomontage
- Significantly affected photomontage
- Land potentially required during construction
- ZTV construction

Operation phase



- Route in tunnel
- Route on surface
- Community area boundary
- Non significantly affected viewpoint
- Significantly affected viewpoint
- Non significantly affected photomontage
- Significantly affected photomontage
- ZTV operation year 1

Viewpoint 306-02-005: view west from Bellsmithy, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in properties bordering the A530 Nantwich Road at Hopley House, Bellsmithy and Yewtree Farm, Manor Cottage, users of Footpath Wimboldsley 3/1, Bridleway Wimboldsley 2/1 and road users of A530 Nantwich Road.

Winter view (baseline)

Date taken: 13/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 07/09/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	37 369048, 362954	
Value of the viewpoint:	This viewpoint has a medium value, its composition being representative and typical of the open and predominantly agricultural to the south of Middlewich, with the A530 Nantwich Road a noticeable component of the view.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the SES1 scheme:	Residents and footpath users have a strong interest in their visual environment and have a high susceptibility to visual change arising from the construction and operation of the SES1 scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.	High



Viewpoint 306-02-005: view west from Bellsmithy, A530 Nantwich Road

Visual baseline description

Winter	The view looks west across the A530 Nantwich Road, with a near distance comprising managed roadside and field boundary hedges with infrequent, isolated roadside trees. Views from properties bordering the A530 Nantwich Road and from footpaths are typically open and unrestricted, although those from Yewtree House and Yewtree Farm to the north are enclosed by garden vegetation. In the middle-distance are large arable fields bordered by managed hedgerows with infrequent trees and occasional small, isolated copses. Overhead power lines are visible against the skyline. The view extends unrestricted to distant field boundary vegetation and woodland belts bordering the River Weaver. Overhead line equipment and train movements along the West Coast Main Line (WCML) are visible against this backdrop but do not break the skyline. The rooflines of Wimboldsley Hall and Wimboldsley Grange are both visible in the far distance.
Summer	In summer the extent of views does not substantially change, although summer foliage on tree canopies in the vicinity of Yewtree House/Yewtree Farm and Hopley House, heavily filters views west from these properties. The WCML and power lines remain evident across much of the view. Individual field and hedgerow trees appear more prominent and distinctive.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties. There is a far-distance skyglow visible above the town of Winsford, approximately 3km to the north-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a major adverse (significant) effect. This would be a result of large-scale construction works associated with the Crewe North Rolling Stock Depot (RSD), Walley's Green embankment and the A530 Nantwich Road satellite compound, in the near and middle-distance of the view and the proximity of the borrow pit including the Borrow Pit A satellite compound. The construction works would be visible across the majority of the view and out of character within the open agricultural setting of the view. During construction, the enhancement of landscape mitigation at Walley's Green embankment (SES1-002-001) will not change the visual effect as described in the main ES. This is because activity to construct the landscape earthworks will be comparable to that of the original scheme, although the reduction in the gradient of the slope will increase the area required for construction. However, the design change will not be discernible when considered in the context of the large-scale construction works of the original scheme. Therefore, the design change will not give rise to a different significant effect, and the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Construction night-time	The main ES reported a major adverse (significant) effect. This would be a result of night-time lighting associated with the Crewe North RSD satellite compounds 1 and 2, A530 Nantwich Road satellite compound and MA02 Borrow Pit A satellite compound introducing additional light sources into the near distance and far-distance views. During construction, the design change will not provide any screening of night-time lighting associated with construction. Therefore, the design change will not give rise to a different significant effect, and the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a major adverse (significant) effect. This would be a result of the introduction of the large-scale infrastructure across the majority of the view, including Walley's Green embankment, Crewe North RSD and the A530 Nantwich Overbridge. At year 1, the design change, will slightly change the visual effect at this viewpoint due to a combination of a more natural profile to the outer slope of Walley's Green embankment and the relocation of the upper edge of the earthwork closer to receptors. This will better integrate the new embankment in views and increase its effectiveness as a visual screen. More of the structures of the SES1 scheme will be screened than in the original scheme but due to the large scale of the new structures, the change in the view will be small. Landscape mitigation planting proposed as part of the original scheme, and in association with landscape earthworks would not be sufficiently mature to assist in the visual integration of the Walley's Green embankment or the visual screening of the SES1 scheme. Therefore, the design change will give rise to a different significant effect, however the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of the presence of operational lighting associated with the Crewe North RSD introducing lighting elements in the middle-distance of the view, in a predominantly rural and unlit area. At year 1, the design change will not change the night-time visual effect at this viewpoint. This is because landscape mitigation planting, in association with landscape earthworks, will not be sufficiently mature to provide any visual screening of light sources. Therefore, the design change will not give rise to a different significant effect, and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
Year 15	Summer	The main ES reported a moderate adverse (significant) effect. This would be a result of a combination of landscape earthworks and maturing mitigation planting which would partially screen Walley's Green embankment, Crewe North RSD and the A530 Nantwich Overbridge and help integrate the original scheme into views across the wider landscape. However, the original scheme, most notably the Crewe North RSD would remain visible across the majority of the view, with some elements visible above the line of mitigation planting. At year 15, the design change will slightly change the visual effect at this viewpoint, due to the maturing and increased depth of the landscape mitigation planting, in combination with the landscape earthworks, which will better integrate Walley's Green embankment into views and filter views north-west of the SES1 scheme. However, the large-scale of the Crewe North RSD, will mean that the SES1 scheme remains visible across the majority of the view above the establishing tree canopy. The SES1 design change will therefore give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of operational lighting associated with the Crewe North RSD, noticeable in the middle-distance of the view in a predominantly rural and unlit area. However, views of lighting would be partially screened by maturing landscape mitigation planting. At year 15, the design change will not change the visual effect at this viewpoint due to the continued presence of tall light columns and presence of skyglow. Maturing landscape mitigation planting in association with the landscape earthworks will screen light sources at a lower height and within the Crewe North RSD. However, lighting will remain noticeable in the middle-distance of the view. Therefore, the design change will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)

Viewpoint 306-02-005: view west from Bellsmithy, A530 Nantwich Road

Visual impact assessment continued

		Permanent effects during operation	Significance of effect
Year 30	Summer	The main ES reported a moderate adverse (significant) effect. This would be a result of mature mitigation planting in association with landscape earthwork, further integrating the A530 Nantwich Road into views of wider landscape and provide a denser screen to views of the Crewe North RSD, although the roofline of the maintenance shed would remain apparent above the tree canopies. At year 30, the design change will slightly change the visual effect at this viewpoint due to the greater maturity and depth of landscape mitigation planting in combination with the landscape earthworks. The effect will be to integrate Walley's Green embankment into views and further filter views of the SES1 scheme. The roofline of the Crewe North RSD will remain apparent above the maturing tree canopy. The design change will therefore give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of operational lighting associated with the Crewe North RSD remaining noticeable in the middle-distance of the view in a predominantly rural and unlit area, with some views partially screened by mature landscape mitigation planting. At year 30, the design change will not change the visual effect at this viewpoint. This is because the greater maturity of landscape mitigation planting in association with the landscape earthworks will not provide additional screening of light sources at the Crewe North RSD. Tall lighting will remain noticeable in the middle-distance of the view above the tree canopy. Therefore, the design change will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

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Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Winter view (baseline)

Date taken: 27/03/2018. Time taken: 11:55



Summer view (baseline)

Date taken: 08/10/2018. Time taken: 09:23



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	368921.837, 363619.671	
Elevation:	49.742m AOD	
Value of the viewpoint:	This viewpoint has a medium value, its composition being representative and typical of the open and predominantly agricultural landscape to the south of Middlewich, with the A530 Nantwich Road a noticeable component of the view.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the SES1 scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the SES1 scheme. The viewpoint is also representative of road users and staff and pupils at the school of lower susceptibility, whose attention is less focused on the landscape.	High



Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

Visual baseline description

Winter	In the near distance is a gated field access, set within a continuous roadside managed hedge with infrequent hedge trees. The views from School Cottages and from the public footpath are unrestricted across large arable fields, bordered by managed hedges with intermittently spaced trees and isolated field trees. The wide field of view extends to a middle-distance skyline of hedge trees, copses and woodland belts. Overhead line equipment and train movements on the WCML are evident across much of the middle distance, set against the wooded backdrop. Overhead power lines and pylons punctuate the skyline in the middle distance, along with the roofline of Wimboldsley Grange. The Peckforton Hills form the far-distant horizon. Views west from the adjacent grounds and buildings of Wimboldsley Community Primary School are partially obstructed by the line of the hedge and by School Cottages alongside the A530 Nantwich Road. Views from the rear of properties on Lea Drive are heavily screened by garden and roadside tree planting.
Summer	In summer, the extent of view does not substantially change. The WCML, pylons and gantries remain evident across much of the view. Field boundary hedgerows provide a small degree of additional screening and mature field trees in foliage are more distinctive in terms of size and form.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties. Distant skyglow from the town of Winsford is evident approximately 3km to the north-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a major adverse (significant) effect. This would be a result of the introduction of large-scale construction works, including the construction of Crewe North RSD, Walley's Green embankment and MA02 Borrow Pit A and B to the south and north, into near and middle-distance views. During construction, the enhancement of landscape mitigation at Walley's Green embankment (SES1-002-001) will not change the visual effect as described in the main ES. This is because activity to construct the landscape earthworks and soften the outer slope of the Walley's Green embankment will be comparable to that of the original scheme, although the reduction in the gradient of the slope will increase the area required for construction of the earthwork. The design change will not be discernible when considered in the context of the large-scale construction works of the original scheme. Therefore, the design change will not give rise to a different significant effect. The level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Construction night-time	The main ES reported a major adverse (significant) effect. This would be a result of the presence of night-time lighting associated with the Crewe North RSD satellite compounds 2 and 3, introducing additional light sources into middle-distance views. During construction the design change will not change the night-time visual effect at this viewpoint, as no additional lighting will be introduced as part of the SES1 amendment. Therefore, the design change will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a major adverse (significant) effect. This would be a result of the introduction of overhead line equipment, passing trains, Walley's Green embankment and buildings within the Crewe North RSD, across the majority of the view to the west. For some receptors, the original scheme would form the new skyline. At year 1, the SES1 design change will slightly change the visual effect at this viewpoint due to a combination of a more natural profile to the outer slope of the Walley's Green embankment and the relocation of the top of the earthwork closer to the receptors. This will better integrate the new embankment in, and bring the edge of the earthwork closer, appearing to marginally lift the edge of the earthwork and with it, the screening within views west. However, due to the large scale of the Crewe North RSD area, there will only be a slight change in the view at year 1. This is due to the landscape earthworks in combination with landscape mitigation planting proposed as part of the original scheme, which will not be sufficiently mature to assist in the visual integration of Walley's Green embankment or the visual screening of the SES1 scheme. Therefore, the design change will give rise to a different significant effect however the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Night-time	The main ES reported a major adverse (significant) effect. This would be due to the introduction of lighting associated with the Crewe North RSD in the middle distance. At year 1, the SES1 design change will not change the night-time visual effect at this viewpoint as landscape mitigation planting, in association with landscape earthworks, will not be sufficiently mature to provide any visual screening of light sources. Therefore, the design change will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
Year 15	Summer	The main ES reported a moderate adverse (significant) effect. This would be a result of landscape mitigation planting proposed as part of the original scheme, partially screening overhead line equipment and built structures within Crewe North RSD. Tall mast lighting columns would, however, remain visible on the skyline. At year 15, the SES1 design change will slightly change the visual effect at this viewpoint. Maturing landscape mitigation planting proposed as part of the original scheme and increased in depth by the SES1 design change will provide some screening and better integration of Walley's Green embankment. However, tall mast lighting columns will remain visible on the skyline above the mitigation planting provided by the original scheme. Therefore, the SES1 design change will give rise to a different significant effect however the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a major adverse (significant) effect. This would be a result of night-time lighting associated with the Crewe North RSD in the middle distance of the view, above establishing mitigation planting provided as part of the original scheme. At year 15, the SES1 design change will slightly change the visual effect at this viewpoint. Maturing landscape mitigation planting proposed as part of the original scheme and increased in depth by the SES1 design change will provide additional screening of low level light sources at Crewe North RSD. However, tall lighting will remain visible in the middle-distance of the view. Therefore, the SES1 design change will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

Visual impact assessment continued

		Permanent effects during operation	Significance of effect
Year 30	Summer	The main ES reported a moderate adverse (significant) effect. This would be a result of landscape mitigation planting proposed as part of the original scheme, screening overhead line equipment, and the majority of built structures within Crewe North RSD. Tall mast lighting columns would remain visible on the skyline. At year 30, the SES1 design change will slightly change the visual effect at this viewpoint. Mature landscape mitigation planting proposed by the original scheme will be increased in depth by the SES1 design change in combination with the landscape earthworks. This will better integrate Walley's Green embankment into views, raising the height of screen planting within views of the original scheme. However, tall mast lighting columns will remain visible on the skyline above the mitigation planting provided by the original scheme and extended by the SES1 design change. Therefore, the SES1 design change will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of night-time lighting associated with Crewe North RSD in the middle distance of the view, above establishing mitigation planting provided as part of the original scheme. At year 30, the SES1 design change will slightly change the visual effect at this viewpoint. This is due to greater maturity of landscape mitigation planting proposed as part of the original scheme and increased in depth by the design change. The effect of this will provide further screening of low level light sources at Crewe North RSD. However, tall lighting will remain visible in the middle-distance of the view. Therefore, the SES1 design change will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Current baseline - winter view

Date taken: 27/03/2018. Time taken: 11:55



Winter verifiable photomontage - construction



The viewpoint has been taken approximately 310m away from the SES1 scheme. Viewpoint location shown on Map LV-07-307. For full details of the visual assessment at viewpoint 307-02-002 refer to SES1 and AP1 ES Volume 5, Appendix: LV-001-0MA02 Part 3.

This verifiable photomontage (Type 4 as described in Visual Representation of Development Proposals LI TGN 06/19) provides an illustration of how the SES1 scheme may look during the peak construction to help inform the visual impact assessment. The construction methods and siting of construction activities of the SES1 scheme may be subject to change in response to consultation and ongoing design. The extent of land required temporarily to construct the SES1 scheme will not extend beyond that shown in the photomontage. Changes in the construction of the SES1 scheme will not result in any significant adverse change in the environmental effects reported in the assessment.

Each individual image represents a 39.6° horizontal field of view with planar projection. At this scale the images do not lend themselves to direct comparison out in the field. Therefore, for viewing in the field, it is recommended that each image from the panoramic photomontage is printed individually, onto an A3 landscape sheet (image size 390mm x 260mm) to be viewed at a comfortable arms length. For further details on the selection of photomontage locations, verifiable methodology and presentation refer to the Landscape and visual assessment Technical Note - Approach to verifiable photomontages (SMR Volume 5, Appendix: CT-001-00001) of the main ES.

Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens		
Approximate GPS co-ordinates ref.	368921.837, 363619.671	Direction of View:	264.95°
Elevation:	49.742m AOD	Height of Camera:	1.629m

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	Map Name Verifiable Photomontage Peak Construction Phase - Winter Viewpoint 307-02-002	
Doc Number: 2PT24-MWJ-EV-MAP-M000-004146-P01		Date: Jan 2022

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Current baseline - winter view

Date taken: 27/03/2018. Time taken: 11:55



Winter verifiable photomontage - operation year 1



The viewpoint has been taken approximately 310m away from the SES1 scheme. Viewpoint location shown on Map LV-08-307. For full details of the visual assessment at viewpoint 307-02-002 refer to SES1 and AP1 ES Volume 5, Appendix: LV-001-0MA02 Part 3.

This verifiable photomontage (Type 4 as described in Visual Representation of Development Proposals LI TGN 06/19) provides an illustration of how the SES1 scheme may look in 2038 (opening year) to help inform the visual impact assessment. The design of the SES1 scheme may be subject to design development in response to consultation. Development of detail design after SES1 submission will not result in any significant adverse change in the environmental effects reported in the assessment. Where new planting is proposed, it has been shown as immature plants which would mature over time to further integrate the SES1 scheme into the landscape.

Each individual image represents a 39.6° horizontal field of view with planar projection. At this scale the images do not lend themselves to direct comparison out in the field. Therefore, for viewing in the field, it is recommended that each image from the panoramic photomontage is printed individually, onto an A3 landscape sheet (image size 390mm x 260mm) to be viewed at a comfortable arms length. For further details on the selection of photomontage locations, verifiable methodology and presentation refer to the Landscape and visual assessment Technical Note - Approach to verifiable photomontages (SMR Volume 5, Appendix: CT-001-00001) of the main ES.

Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens		
Approximate GPS co-ordinates ref.	368921.837, 363619.671	Direction of View:	264.95°
Elevation:	49.742m AOD	Height of Camera:	1.629m

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	Map Name Verifiable Photomontage Operation Year 1 (2038) - Winter Viewpoint 307-02-002	
	Community Area MA02: Wimboldsley to Lostock Gralam	
Doc Number: 2PT24-MWJ-EV-MAP-M000-004145-P01		Date: Jan 2022

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Current baseline - summer view

Date taken: 08/10/2018. Time taken: 09:23



Summer verifiable photomontage - operation year 15



The viewpoint has been taken approximately 310m away from the SES1 scheme. Viewpoint location shown on Map LV-08-307. For full details of the visual assessment at viewpoint 307-02-002 refer to SES1 and AP1 ES Volume 5, Appendix: LV-001-0MA02 Part 3.

This verifiable photomontage (Type 4 as described in Visual Representation of Development Proposals LI TGN 06/19) provides an illustration of how the SES1 scheme may look in 2053 (15 years after opening) to help inform the visual impact assessment. The design of the SES1 scheme may be subject to design development in response to consultation. Development of detail design after SES1 submission will not result in any significant adverse change in the environmental effects reported in the assessment. Where new planting is proposed, it has been shown as semi-mature trees which have put on 15 years of growth to illustrate how the SES1 scheme will further integrate into the landscape over time.

Each individual image represents a 39.6° horizontal field of view with planar projection. At this scale the images do not lend themselves to direct comparison out in the field. Therefore, for viewing in the field, it is recommended that each image from the panoramic photomontage is printed individually, onto an A3 landscape sheet (image size 390mm x 260mm) to be viewed at a comfortable arms length. For further details on the selection of photomontage locations, verifiable methodology and presentation refer to the Landscape and visual assessment Technical Note - Approach to verifiable photomontages (SMR Volume 5, Appendix: CT-001-00001).

Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens		
Approximate GPS co-ordinates ref.	368921.837, 363619.671	Direction of View:	264.95°
Elevation:	49.742m AOD	Height of Camera:	1.629m

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	Map Name Verifiable Photomontage Operation Year 15 (2053) - Summer Viewpoint 307-02-002	

Viewpoint 310-02-010: view south-west from King's Lane Farm, King's Lane

This viewpoint is representative of views experienced by residents and road users on the minor road, King's Lane, which provides access to the west of the M6 and connects into the B5081 Middlewich Road.

Winter view (baseline)

Date taken: 30/11/2021 (stitched panorama)



Summer view (baseline)

Date taken: 21/05/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	373514, 370190	
Value of the viewpoint:	This viewpoint has a medium value, its composition being primarily comprised of soft landscape elements with the presence of detracting elements such as the cement works.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the SES1 scheme:	The viewpoint is representative of residential properties and road users. Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the SES1 scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.	High



Viewpoint 310-02-010: view south-west from King's Lane Farm, King's Lane

Visual baseline description

Winter	The view looks south-west across a relatively flat agricultural landscape. The near distance comprises an arable field unit defined by maturing, mixed species planting belts and areas of woodland. Views from the property at King's Lane Farm are partially filtered by the dense, mature roadside hedge and deciduous hedgerow trees. There are more open views through gaps in the hedge at gateways of the landscape beyond the hedge. A works compound, including large industrial buildings, is partially visible in the west.
Summer	In summer the extent of views will be reduced by the presence of summer foliage on the roadside hedge along the southern boundary of King's Lane and tree canopies in the vicinity of King's Lane Farm. The works compound and associated buildings remain evident to the west. Individual field and hedgerow trees appear more prominent and distinctive.
Night-time	Local roads in the area are unlit, with night-time lighting limited to the FP McCann industrial estate. There is skyglow evident from the village of Byley approximately 1.2km to the north-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a high magnitude of change and a major adverse (significant) effect. This would be a result of the introduction of large-scale construction works associated with MA02 Borrow Pit D, into near-distance views of a largely agricultural landscape. As a result of the SES1 design change removal of MA02 Borrow Pit D, north of Moss Lane (SES1-002-002). MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove a significant effect.	Negligible (non-significant)
	Construction night-time	The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of night-time lighting at MA02 Borrow Pit D compound introducing new lighting in the far distance, seen in the context of existing light sources. As a result of the SES1 design change, there will be no requirement for the MA02 Borrow Pit D compound. The SES1 design change will therefore remove a non-significant effect.	Negligible (non-significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be due to the partial restoration of the site of f MA02 Borrow Pit D to its former arrangement. At year 1, as a result of the SES1 design change, MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring views over the landscape in the vicinity of MA02 Borrow Pit D. At year 15, as a result of the SES1 design change, MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation restoring views over the landscape in the vicinity of MA02 Borrow Pit D. At year 30, as a result of the SES1 design change, MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
	Operation cumulative	There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 310-02-011: view north-west from Byway Cranage 7/1, A54 Middlewich Road

This viewpoint is representative of views experienced by residents of Holly House Farm and properties off the A544 Middlewich Road, users of Byway Cranage 7/1 and road users along the minor road and A54 Middlewich Road.

Winter view (baseline)

Date taken: 30/11/2020 (stitched panorama)



Summer view (baseline)

Date taken: 21/05/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	373732, 369002	
Value of the viewpoint:	This viewpoint has a medium value, its composition being primarily comprised of soft landscape elements with some detracting features including the presence of vehicles and infrastructure associated with the M6 road corridor.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the SES1 scheme:	The viewpoint is representative of residential properties and road users. Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the SES1 scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.	High



Viewpoint 310-02-011: view north-west from Byway Cranage 7/1, A54 Middlewich Road

Visual baseline description

Winter	The view is orientated to the north-west across a relatively flat agricultural landscape. The near distance comprises an access gateway, grassland and a mix of mature and recently planted hedgerow with relatively immature planting associated with a farm access track. A number of mature, coniferous trees are visible to the western extent of the view which define the rear garden boundary to Holly House Farm. Beyond the access track, the view comprises arable field units set against interlinked areas and belts of woodland cover which provide continuous tree cover across the skyline. Vehicles associated with the M6 are visible in the east, together with the King's Lane overbridge across the motorway. Views from residential properties and users of the footpath are partially filtered by intervening vegetation.
Summer	In summer the extent of views will be reduced by the presence of summer foliage on the roadside hedge and the hedge and hedgerow trees to the north of the farm access track. The works compound and associated buildings remain evident to the western extent of the view. Woodland areas, individual field and hedgerow trees appear more prominent and distinctive. Generally, views beyond the near-distance vegetation present in the road corridor, field and property boundaries will be less extensive in summer.
Night-time	Lighting associated with vehicles on the M6 will be visible although local roads in the area are unlit with night-time lighting limited to individual properties. There is far-distance skyglow above the town of Rudheath, approximately 6km to the north-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a medium magnitude of change and a moderate adverse (significant) effect. This would be a result of the introduction of large-scale construction works associated with MA02 Borrow Pit D, into partially filtered, middle-distance views of the otherwise largely agricultural landscape. As a result of the design change removal of MA02 Borrow Pit D, north of Moss Lane (SES1-002-002), MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove a significant effect.	Negligible (non-significant)
	Construction night-time	The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of the introduction of night-time lighting at MA02 Borrow Pit D compound creating an area of skyglow in the far distance of the view, seen in the context of existing light sources. As a result of the design change, there will be no requirement for the MA02 Borrow Pit D compound. The design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of filtered views of the site of MA02 Borrow Pit D not yet fully restored to its former condition due to the immaturity of the reinstatement planting. At year 1, as a result of the SES1 design change, the land will be unaffected. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring views over the landscape in the vicinity of MA02 Borrow Pit D. At year 15, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation fully restoring views over the landscape in the vicinity of MA02 Borrow Pit D. At year 30, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
	Operation cumulative	There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 310-04-012: view south from the B5081 Middlewich Road

This viewpoint is representative of views experienced by road users on the B5081 Middlewich Road which provides access between the villages of Byley to the south-west and Swan Green to the north-east and workers in the industrial estate.

Winter view (baseline)

Date taken: 30/11/2021 (stitched panorama)



Summer view (baseline)

Date taken: 21/05/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	372460, 369911	
Value of the viewpoint:	This viewpoint has a medium value, its composition being primarily comprised of soft landscape elements with some detracting features including the presence of vehicles and infrastructure associated with the M6 corridor.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the SES1 scheme:	The viewpoint is representative of road users and workers whose attention is likely to be focused on the immediate landscape. There is a medium to low susceptibility to visual change arising from the construction and operation of the original revised scheme.	Medium - low



Viewpoint 310-04-012: view south from the B5081 Middlewich Road

Visual baseline description

Winter	The view looks south along the B5081 Middlewich Road corridor. The near distance comprises the black topped road surfaces near the junction with the main road access to the electricity substation and associated metal railings to the industrial estate boundary. Beyond the railings there is an area of dense shrub and tree cover which screens views south and east. Views from the B5801 Middlewich Road corridor comprise dense woodland to the east with a mixed species hedge to the western boundary of the corridor. Further to the west the agricultural landscape is more open with field boundaries comprising a mix of hedgerows and hedgerow trees.
Summer	In summer the extent of views will be reduced by the presence of summer foliage on the woodland planting to the east of the B5081, the roadside hedge and individual hedgerow trees. The presence of foliage will further reduce the extent of visibility towards the proposed MA02 Borrow Pit D site area. Woodland areas, individual field and hedgerow trees appear more prominent and distinctive. The extent of hardscape elements visible in the near distance, including the road and pathway surfaces, signage and associated infrastructure will not change in the summer.
Night-time	The night-time baseline is not described for this viewpoint, as night-time effects are considered only for occupants of residential properties, residents staying in hotels and healthcare institutions.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a negligible magnitude of change and a negligible (non-significant) effect. This would be a result of views of construction activity being largely filtered by intervening vegetation. As a result of the SES1 design change Removal of MA02 Borrow Pit D, north of Moss Lane (SES1-002-002) MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove a non-significant effect.	Negligible (non-significant)
	Construction night-time	Night-time effects have only been considered for occupiers of residential properties and residents staying in hotels and healthcare institutions.	Not assessed
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a negligible (non-significant) effect. This would be due to views of the restored land which would have been occupied by MA02 Borrow Pit D in construction, being heavily filtered through intervening vegetation. At year 1, as a result of the SES1 design change the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	Night-time effects have only been considered for occupiers of residential properties and residents staying in hotels and healthcare institutions.	Not assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring filtered views over the landscape in the vicinity of MA02 Borrow Pit D. At year 15, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	Night-time effects have only been considered for occupiers of residential properties and residents staying in hotels and healthcare institutions.	Not assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation restoring filtered views over the landscape in the vicinity of MA02 Borrow Pit D. At year 30, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as night-time effects are only considered for residents and people staying in hotels or healthcare institutions.	Not assessed
	Operation cumulative	There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 310-02-013: view north-east from Moss Lane

This viewpoint is representative of views experienced by residents of properties on Moss Lane and the B5081 to north of Byley, staff and pupils at Byley Primary School, visitors to Lowe’s Farm Shop and by road users along Moss Lane.

Winter view (baseline)

Date taken: 30/11/2021 (stitched panorama)



Summer view (baseline)

Date taken: 21/05/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	372687, 369172	
Value of the viewpoint:	This viewpoint has a medium value, its composition being comprised of farm buildings, soft landscape elements and agricultural field units.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the SES1 scheme:	Residents have a strong interest in their visual environment. They are therefore highly susceptible to visual change arising from the construction and operation of the SES1 scheme. Staff and pupils at Byley Primary School, road users and customers have a lower susceptibility as their attention is less focused on the landscape.	High



Viewpoint 310-02-013: view north-east from Moss Lane

Visual baseline description

Winter	The view looks north-east across a relatively flat agricultural landscape. In the near distance there is a single storey, agriculturally styled building which houses the Lowe's Farm Shop with associated gravel parking area, ancillary buildings and timber post and rail fence defining the property boundary. To the east of the farm shop, the view is dominated by predominantly grassed, pastoral field units structured by a range of mature trees to field boundaries and in some cases more substantial tree belts. This tree cover is assimilated in the view to provide almost continuous cover across the skyline. The tarmac surface of Moss Lane, with associated occasional mature trees to the highway boundary, is also visible to the east of the view and is orientated in an east-west direction. Views from residential properties on Moss Lane north and east are filtered and screened by intervening farm buildings and existing vegetation. Views east from residential properties on B5081 Byley Road are partly filtered by intervening vegetation.
Summer	In summer the individual field and hedgerow trees appear more prominent and distinctive and reduce the extent of views across the agricultural landscape to the north.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties and the farm shop. There is far-distance skyglow evident from operational and security lighting associated with the concrete works, lighting from the M6 corridor and from the village of approximately 2.5km to the north-east.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
Construction		The main ES reported a medium magnitude of change and a moderate adverse (significant) effect. This would be a result of the removal of existing vegetation and the introduction of new landform and earthworks within filtered views across the agricultural landscape to the north. During construction, as a result of the SES1 design change Removal of Borrow Pit D, north of Moss Lane (SES1-002-002), MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove a significant effect.	Negligible (non-significant)
Construction night-time		The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of the introduction of night-time lighting at MA02 Borrow Pit D compound creating an area of skyglow in the far distance of the view, seen in the context of existing light sources. At year 1, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
Construction cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a negligible (non-significant) effect. This would be due to views of the site of the MA02 Borrow Pit D being heavily filtered through intervening vegetation. At year 1, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring filtered views over the rural landscape in the vicinity of MA02 Borrow Pit D. At year 15, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the greater maturity of reinstatement field boundary vegetation fully restoring filtered views over the rural landscape in the vicinity of MA02 Borrow Pit D. At year 30, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 310-02-014: view north east from the B5081 Middlewich Road/Moss Lane/Drakelaw Lane

This viewpoint is representative of views experienced by residents in Byley (off Drakelaw Lane) and road users on the B5081/Moss Lane/Drakelaw Lane.

Winter view (baseline)

Date taken: 30/11/2021 (stitched panorama)

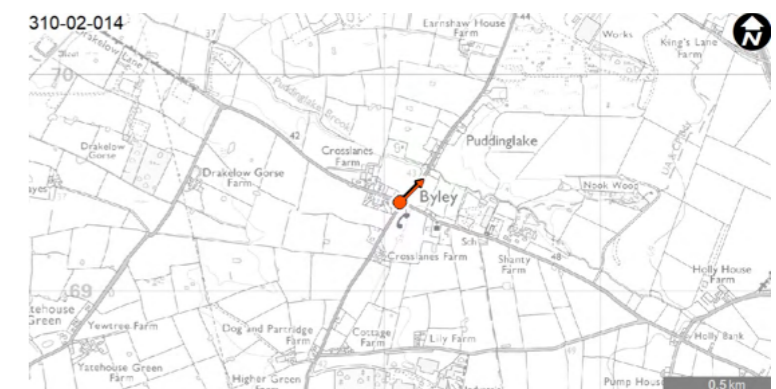


Summer view (baseline)

Date taken: 21/05/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	372076, 369412	
Value of the viewpoint:	This viewpoint has a medium value, its composition being comprised of rural roads, partial views of residential properties and soft landscape elements.	Sensitivity of the receptor: High
Susceptibility of the receptor to the change arising from the SES1 scheme:	The attention of residents is more likely to be focused on views of the landscape beyond their property boundary, while the attention of road users is likely to be on the immediate landscape. There is a high susceptibility to visual change arising from the construction and operation of the SES1 scheme.	



Viewpoint 310-02-014: view north east from the B5081 Middlewich Road/Moss Lane/Drakelaw Lane

Visual baseline description

Winter	The view looks north-east over the crossroads at the junction of the B5081 Byley Road with Moss Lane/Drakelaw Lane. The near distance comprises the tarmac road surfaces, grass verges and assorted boundary treatments to the highway boundary including a metal railing and hedgerows. Associated highways infrastructure including streetlights and signage is also visible. Beyond the highway boundary, to the north-east, a small pastoral field unit is visible enclosed with mature hedgerows and hedgerow trees and including a wood pole and wire overhead cable. The roofscape of properties in Byley is partially visible in views filtered by mature trees.
Summer	In summer the extent of views is reduced by the presence of summer foliage on the mature trees and woodland cover alongside the B5081 Middlewich Road and west of Byley. Woodland areas, individual field and hedgerow trees appear more prominent and distinctive. The extent of hardscape elements visible in the near distance, including the road and pathway surfaces, signage and associated infrastructure will not change in the summer.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties in Byley and occasional vehicles. There is far-distance skyglow from several local settlements within approximately 5km to the north-west and north.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of views of the MA02 Borrow Pit D site area and associated construction activity being heavily filtered by dense areas of woodland and tree cover in the intervening landscape. During construction, as a result of the SES1 design change Removal of Borrow Pit D, north of Moss Lane (SES1-002-002), MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Construction night-time	The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of the introduction of night-time lighting at MA02 Borrow Pit D compound creating an area of skyglow above intervening vegetation. As a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a negligible (non-significant) effect. This would be a result of the restoration of the land affected by construction in the vicinity of MA02 Borrow Pit D and the filtering effect of existing intervening vegetation. At year 1, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring views over the landscape in the vicinity of MA02 Borrow Pit D. At year 15, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring views over the landscape in the vicinity of MA02 Borrow Pit D. At year 30, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 310-02-015: view west from King's Lane

This viewpoint is representative of views experienced by residents of properties situated on King's Lane and hotel guests and golfers at Woodside Golf Club. The viewpoint is also representative of road users on the M6 and on King's Lane.

Winter view (baseline)

Date taken: 24/03/2021 (stitched panorama)



Summer view (baseline)

Date taken: 21/05/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	373953, 369619	
Value of the viewpoint:	This viewpoint has a medium value, its composition being comprised of agricultural fields and soft landscape elements, interrupted by near-distance traffic movement on the M6.	Sensitivity of the receptor: Medium-high
Susceptibility of the receptor to the change arising from the SES1 scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the SES1 scheme. Hotel guests, golfers and road users have lower susceptibility as their attention is less focused on the landscape.	



Viewpoint 310-02-015: view west from King's Lane

Visual baseline description

Winter	The view is located on King's Lane, adjacent with Woodside Golf Club and looking west across the M6. The near distance is occupied by the northbound and southbound highways of the M6, with a high frequency of traffic movement across the line of view. Timber boundary fencing runs alongside the motorway corridor, with limited roadside vegetation and occasional signage gantries visible. The middle distance and far distance comprise an open agricultural landscape of broad fields, flanked by distant blocks of woodland that create a low skyline. More distant views from residential properties and hotel accommodation on King's Lane are heavily filtered by a combination of golf course vegetation, hedgerows and a belt of mature woodland bordering King's Lane. Recreational users of the golf course have some relatively open views across and over the motorway; other views are partially screened by roadside hedges bordering King's Lane.
Summer	In summer the extent of views for residents and hotel users will be reduced by the presence of summer foliage on hedgerows, mature trees within the golf course and woodland cover flanking King's Road. The M6 corridor will remain a prominent feature in the near distance.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties in Byley and passing vehicles using the M6. There is far-distance skyglow from several local settlements approximately 5km to the north-west and north.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
Construction		The main ES reported a low magnitude of change and a minor adverse (non-significant) effect. This would be a result of the filtering of views towards the north-eastern extents of MA02 Borrow Pit D and construction vehicle movements seen in the context of traffic movements along the M6 road corridor. As a result of the design change Removal of Borrow Pit D, north of Moss Lane (SES1-002-002), MA02 Borrow Pit D will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
Construction night-time		The main ES reported a negligible (non-significant) effect. This would be a result of the introduction of night-time lighting at MA02 Borrow Pit D compound in the far distance of the view seen in the context of existing light sources. As a result of the SES1 design change, MA02 Borrow Pit D compound will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
Construction cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a negligible (non-significant) effect. This would be a result of the restoration of the land affected by construction in the vicinity of MA02 Borrow Pit D and the filtering effect of existing intervening vegetation. At year 1, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation largely restoring filtered views over the landscape in the vicinity of MA02 Borrow Pit D. At year 15, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of the maturing, reinstatement field boundary vegetation restoring filtered views over the landscape in the vicinity of MA02 Borrow Pit D. At year 30, as a result of the SES1 design change, the land will no longer be required. The SES1 design change will therefore remove visual effects at this viewpoint.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

SES1 revised scheme: non-significantly affected viewpoints: construction phase

Table 1: SES1 revised scheme schedule of non-significantly affected viewpoints, with justification, construction

Viewpoint number	Construction	Construction night-time	Construction cumulative	Justification
310-02-010	Negligible	Negligible	No cumulative effect	The SES1 design change will remove construction effects arising as a result of Borrow Pit D
310-02-011	Negligible	Negligible	No cumulative effect	The SES1 design change will remove construction effects arising as a result of Borrow Pit D
310-04-012	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove construction effects arising as a result of Borrow Pit D
310-02-013	Negligible	Negligible	No cumulative effect	The SES1 design change will remove construction effects arising as a result of Borrow Pit D
310-02-014	Negligible	Negligible	No cumulative effect	The SES1 design change will remove construction effects arising as a result of Borrow Pit D
310-02-015	Negligible	Negligible	No cumulative effect	The SES1 design change will remove construction effects arising as a result of Borrow Pit D

SES1 revised scheme: non-significantly affected viewpoints: operation phases

Table 2: SES1 revised scheme schedule of non-significantly affected viewpoints, with justification, operation

Viewpoint number	Operation year 1 (2038)		Operation year 15 (2053)		Operation year 30 (2068)		Operation cumulative	Justification
	Winter	Night-time	Summer	Night-time	Summer	Night-time		
310-02-010	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove operation effects arising as a result of Borrow Pit D
310-02-011	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove operation effects arising as a result of Borrow Pit D
310-04-012	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove operation effects arising as a result of Borrow Pit D
310-03-013	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove operation effects arising as a result of Borrow Pit D
310-02-014	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove operation effects arising as a result of Borrow Pit D
310-02-015	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The SES1 design change will remove operation effects arising as a result of Borrow Pit D

3.3 SES1 revised scheme assessment matrices

SES1 revised scheme landscape assessment matrix

3.3.1 Table 3 below summarises the assessment of significance for the LCA assessed in the Wimboldsley to Lostock Gralam area (MA02). These are ordered from south to north along the route of the SES1 revised scheme. The assessment of significant effects is presented in SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02), Sections 3 and 4.

Table 3: SES1 revised scheme landscape assessment matrix summarising the assessment of significance for all of the LCA affected by SES1 revised scheme identified in the Wimboldsley to Lostock Gralam

LCA	Construction	Construction cumulative	Operation year 1 (2038)	Operation year 15 (2053)	Operation year 30 (2068)	Operation cumulative
Cranage and Byley Sandy Woods	Negligible	No cumulative effect	Negligible	Negligible	Negligible	No cumulative effect
Rudheath Woods, Shakerley and New Platt Meres Sandy Woods	Negligible	No cumulative effect	Negligible	Negligible	Negligible	No cumulative effect

SES1 revised scheme visual assessment matrix

3.3.2 Table 4 below summarises the assessment of significance for all the representative viewpoints identified in the Wimboldsley to Lostock Gralam area (MA02). These are ordered from south to north along the route of the SES1 revised scheme. The assessment of significant effects is presented in SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02), Sections 3 and 4. The night-time assessment (reported in Part 3 of this document, as appropriate) has only been undertaken for certain receptors with a view of proposed continuous lighting during either construction or operation. Further detail on this is provided within the Technical Note: Approach to night-time assessment, contained within the SMR. In most cases, in urban areas, additional lighting is not considered to give rise to significant effects due to the widespread presence of street lighting, lightspill from adjacent buildings and skyglow. Where there is no direct foreground visibility of additional lighting, no further assessment has been undertaken.

Table 4: SES1 revised scheme visual assessment matrix summarising the assessment of significance for all of the viewpoints affected by SES1 revised scheme identified in the Wimboldsley to Lostock Gralam

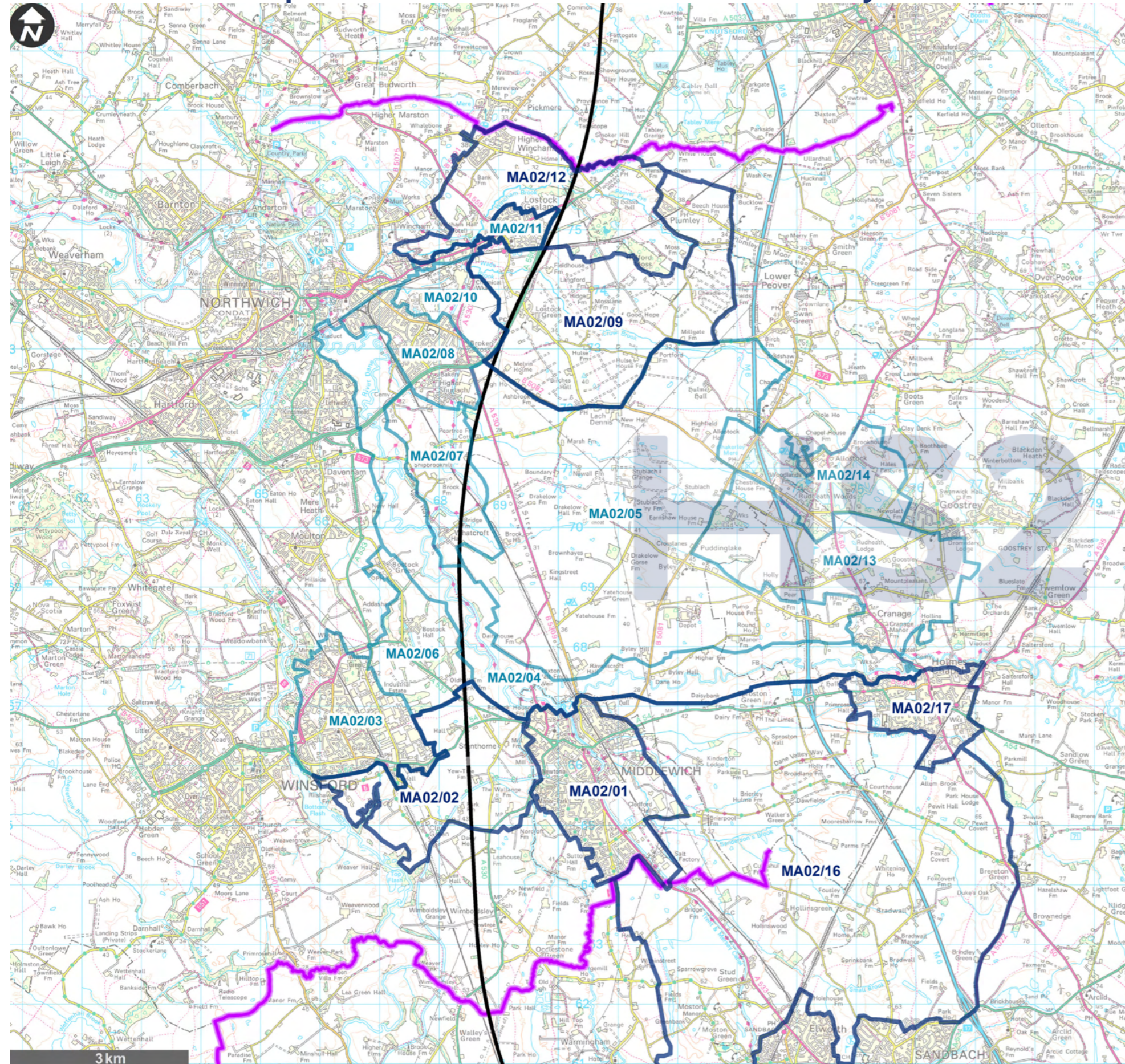
Viewpoints		Construction			Operation year 1 (2038)		Operation year 15 (2053)		Operation year 30 (2068)		Operation cumulative
		Winter	Night-time	Cumulative	Winter	Night-time	Summer	Night-time	Summer	Night-time	
306-02-005	View west from Bellsmithy, A530 Nantwich Road	Major adverse	Major adverse	No cumulative effect	Major adverse	Moderate adverse	Moderate adverse	Moderate adverse	Moderate adverse	Moderate adverse	No cumulative effect
307-02-002	View west from Wimboldsley, A530 Nantwich Road	Major adverse	Major adverse	No cumulative effect	Major adverse	Major adverse	Moderate adverse	Major adverse	Moderate adverse	Moderate adverse	No cumulative effect
310-02-010	View south-west from King's Lane Farm, Kings Lane	Negligible	Negligible	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
310-02-011	View north-west from Byway Cranage 7/1, A54 Middlewich Road	Negligible	Negligible	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
310-04-012	View south from the B5081 Middlewich Road	Negligible	Not assessed	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
310-02-013	View north-east from Moss Lane	Negligible	Negligible	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
310-02-014	View north-east from the B081 Middlewich Road/Moss Lane/ Drakelaw Lane	Negligible	Negligible	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
310-02-015	View west from King's Lane	Negligible	Negligible	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect

Part 4: Additional Provision 1 Environmental Statement

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4.1 Landscape character assessment

Overview of landscape character areas within MA02 affected by AP1 amendments



Landscape character areas assessed as part of AP1 and in the main ES

- MA02/01 - Middlewich
- MA02/02 - Winsford and Middlewich Fringe Farmland
- MA02/09 - Lostock Plain
- MA02/12 - Wincham Brook and Holford Lower Wooded Farmland

Landscape character areas assessed as part of AP1 only

- MA02/16 - Cheshire East/Wimboldsley Plain
- MA02/17 - Holmes Chapel

Landscape character areas assessed in the main ES only

- MA02/03 - Winsford East
- MA02/04 - Dane Valley
- MA02/05 - Stublach Plain
- MA02/06 - Bostock Hall Estate
- MA02/07 - Whatcroft and Billinge Green Flashes
- MA02/08 - Rudheath
- MA02/10 - Rudheath Lime Beds
- MA02/11 - Lostock Gralam
- MA02/13 - Cranage and Byley Sandy Woods
- MA02/14 - Rudheath Woods, Shakerley and New Platt Meres Sandy Woods

Cheshire East/Wimboldsley Plain LCA

Landscape character baseline description

This LCA is part of the broad and largely uniform Cheshire Plain and comprises extensive areas of open and undeveloped countryside. It lies between Holmes Chapel in the east, the Dane Valley in the north, Middlewich in the west and Sandbach to the south.

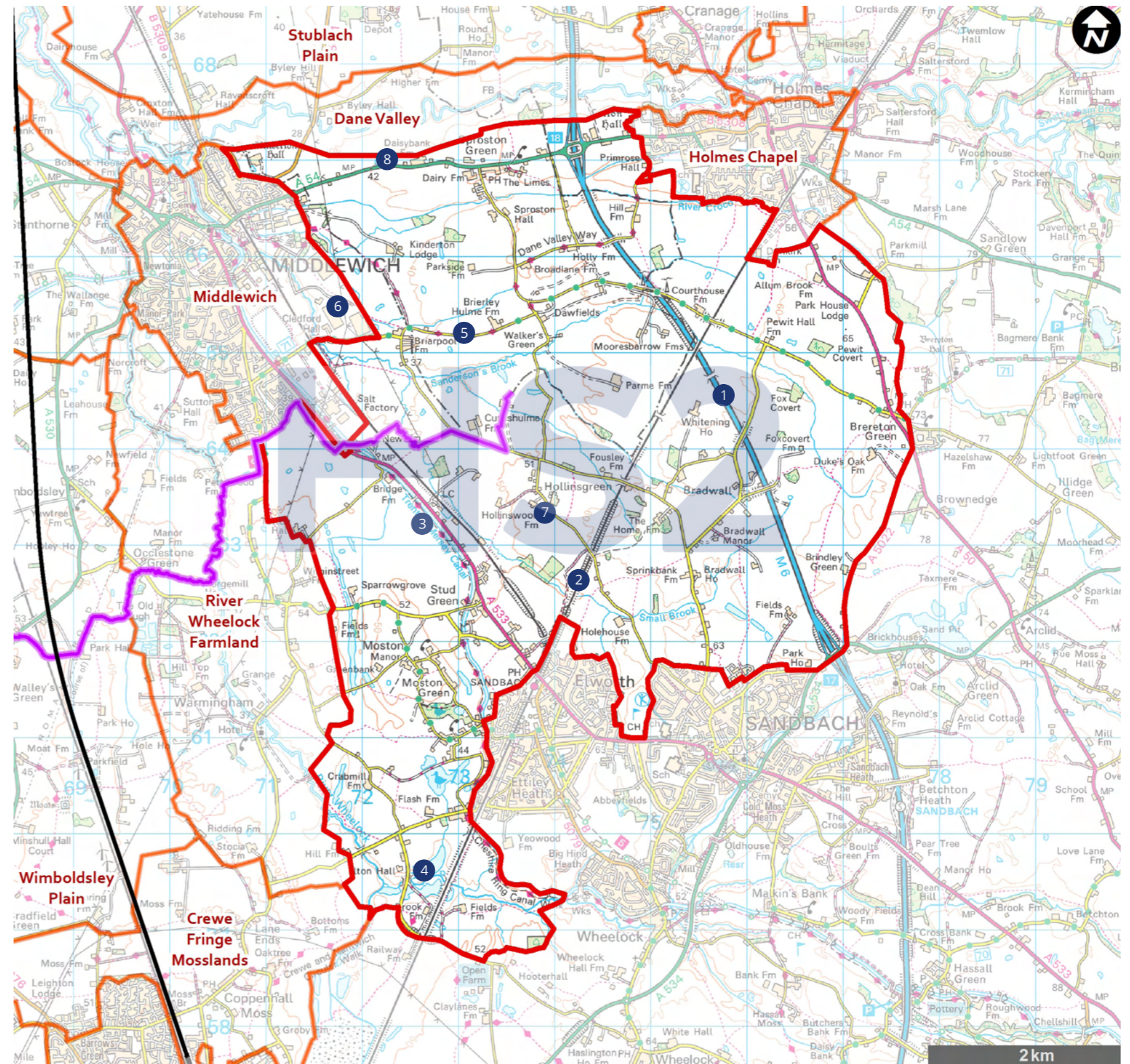
The landscape is relatively flat, with small to medium sized irregular fields of mainly grazing pasture. There is sparse woodland cover but the pattern of hedgerows and frequent hedgerow trees and field trees are characteristic of the Cheshire Plain landscape.

A number of watercourses, including Sanderson's Brook and Small Brook, cross the landscape, with narrow belts of trees indicating their alignment. The many ponds and marl pits in agricultural fields are generally marked by small clumps of trees. The Trent and Mersey Canal runs through the LCA and the Cheshire Ring Canal walk follows its tow path. There are larger areas of open water, designated as wetland nature reserves including Watch Lane Flash Nature Reserve and Elton Flashes Nature Reserve west of Sandbach, in the southern end of the LCA. Development is limited to hamlets, scattered farmsteads and isolated dwellings, linked by narrow, country lanes. Overhead power lines crossing the western part of the LCA, detract from the rural character of the landscape. Transport infrastructure, including the M56 and the Crewe to Manchester Railway line, cross the landscape and reduce tranquillity locally.



Key landscape characteristics

The pattern of hedgerows, trees, watercourses, and water bodies create a distinctive rural landscape. Detracting features including road, rail and power infrastructure sever the landscape and reduce tranquillity locally.

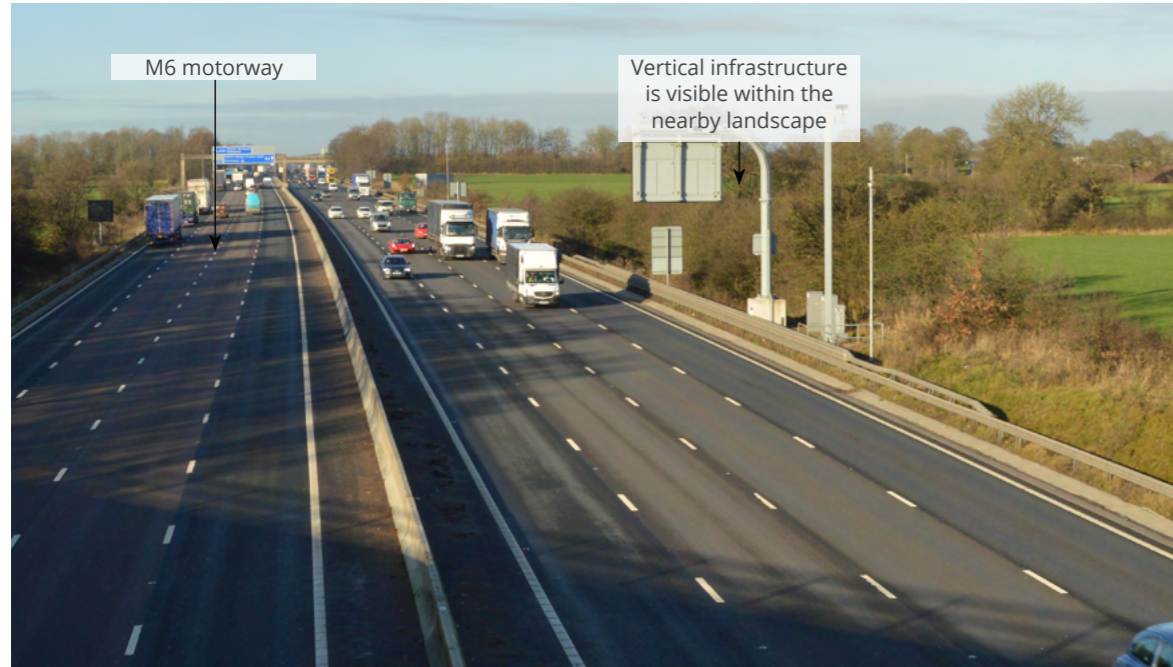


- Route on surface
- LCA boundary
- Community area
- Route in tunnel
- 1 M6
- 2 Crewe to Manchester Railway line
- 3 Trent and Mersey Canal
- 4 Watch Lane and Elton Flashes Nature Reserve
- 5 Dane Valley Way/Sustrans Route 5
- 6 Midpoint 18 Business Park
- 7 Solar farm
- 8 A54

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Key landscape value attributes

Key landscape value attributes of the Cheshire East/Wimboldsley Plain LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

This is an attractive, gently undulating landscape of isolated farmsteads and fields with well-maintained hedges and abundant trees. While transport infrastructure is present, its detracting influence is contained by the layers of vegetation provided by the pattern of the landscape.



Cultural, social and historic

The LCA is predominantly pastoral. The flat landform combined with the pattern of hedgerows and numerous trees provides a sense of enclosure and local tranquillity, away from the transport corridors crossing the LCA and the urban development beyond the boundaries of the LCA. Several bodies of water, including Watch Lane Flash and Elton Flash west of Sandbach, contribute to the landscape setting of Sandbach.

Key landscape characteristics susceptible to the AP1 revised scheme



Overall landscape value

This part of the Cheshire Plain is an attractive landscape of pastoral fields, mature trees, hedgerows, sinuous watercourses, and wetlands. It is mainly tranquil and despite its limited woodland cover, feels secluded due to the numerous trees, hedgerows and small copses around waterbodies. The Trent and Mersey Canal and the Cheshire Ring Canal are important recreational resources. The M6 motorway, railways and several overhead power lines along with numerous local roads are noticeable elements and detract from the otherwise rural character.

The value of this LCA is **medium** given the above.

Overall landscape susceptibility

The low-lying landscape, irregular field patterns, abundant mature trees and recreational routes are susceptible to change. However, the existing presence of transport and power infrastructure has diminished parts of the LCA's susceptibility to change. The landscape has a **medium** susceptibility to change resulting from the AP1 revised scheme.

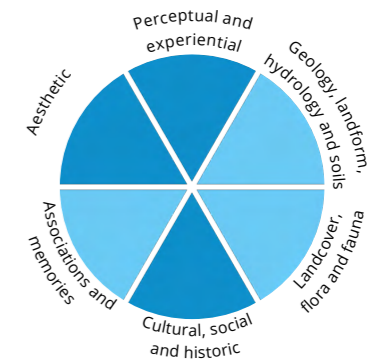
Future baseline

There are no committed developments which will change the baseline.

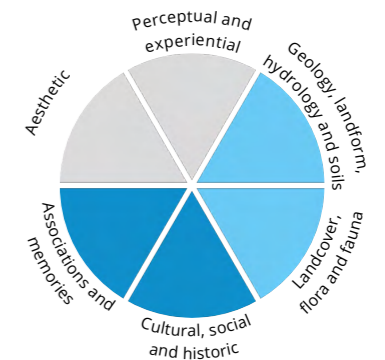
Overall landscape sensitivity

The secluded, rural character of the area, in combination with the susceptibility of landscape features results in the sensitivity of this LCA being **medium**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

This new LCA is located in an area which would not be affected by the original scheme. The AP1 amendment, additional land required for the provision of temporary traffic signals around the M6 junction 18 (AP1-002-008) will introduce temporary traffic management measures including temporary portable traffic lights, on the A54 eastbound approach arm and on the gyratory, operated during peak periods. The AP1 amendment will introduce additional plant and machinery movements into the A54 road corridor. However, these changes will be localised and in keeping with the character of the existing A54 road corridor. The AP1 amendment will require the removal of some roadside trees and the narrowing of the roadside grass verges. Therefore, the magnitude of change will be **low** and there will be a new **minor adverse** (non-significant) effect.

Effects will be **minor adverse (non-significant)**.

Operation year 1

Vegetation lost during construction will have been reinstated but will not be sufficiently mature to fully restore the character of the tree-lined road corridor. However, the change brought about by the AP1 amendment, will be slight and within a localised area of the LCA. Therefore, the magnitude of change will be **negligible** and there will be a new **negligible** (non-significant) effect.

Effects will be **negligible (non-significant)**.

Operation year 15

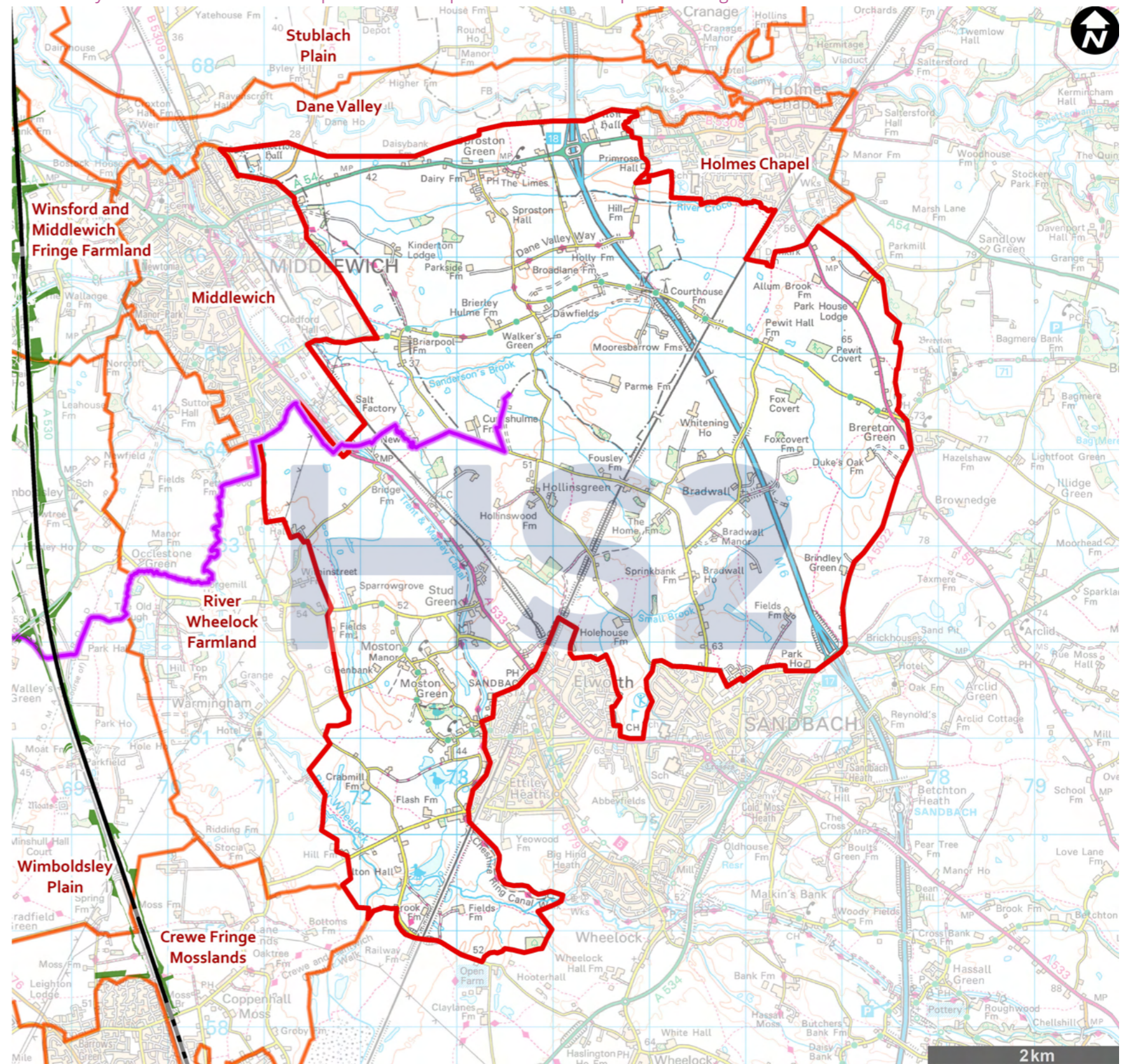
Reinstated roadside vegetation will be sufficiently mature to partially restore the character of the tree-lined road corridor. There will be a barely perceptible change to the character of a localised area of the LCA as a result of the AP1 amendment. The magnitude of change will be **negligible** and there will be a new **negligible** (non-significant) effect.

Effects will be **negligible (non-significant)**.

Operation year 30

Reinstated roadside vegetation will be sufficiently mature to restore the character of the tree-lined road corridor. There will be a barely perceptible change to the character of a localised area of the LCA as a result of the AP1 amendment. The magnitude of change will be **negligible** and there will be a new **negligible** (non-significant) effect.

Effects will be **negligible (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

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Middlewich LCA

Landscape character baseline description

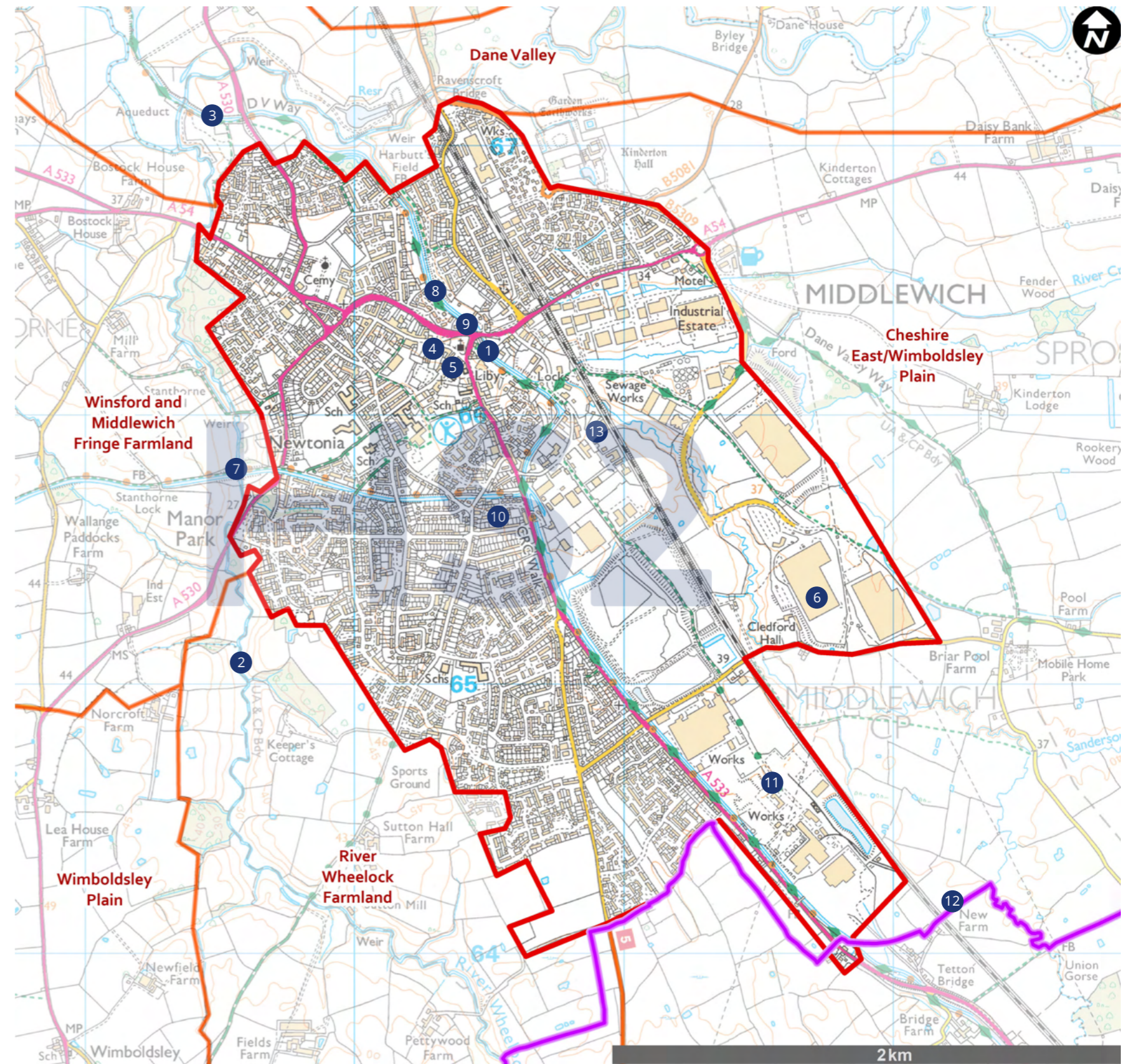
This LCA comprises the town of Middlewich which has historically developed as a centre for salt extraction on the plain between the Dane and Wheelock Rivers.

The town centre consists of largely 19th Century buildings concentrated along Wheelock Street, with the 16th Century Grade II* listed Church of Saint Michaels and All Angels prominent at its eastern end. There are extensive areas of inter-war and post-war housing of primarily semi-detached red and brown brick properties, with relatively few street trees although numerous garden trees. Modern estates of primarily detached red and brown brick properties occupy almost the entire northern and western edges of the town. Riparian woodland borders the western margins of the town along the course of the River Wheelock. The Shropshire Union Canal (Middlewich Branch) joins the Trent and Mersey Canal via the Wardle Lock Branch Canal to the south of the town centre, both of which form tree-lined corridors through the town. Routes through the area include the long-distance footpaths of the Cheshire Ring Canal Walk and Dane Valley Way, several local footpaths, and National Cycle Network Routes 5 and 71. The Sandbach to Northwich railway is a freight only line passing through the town and the former Middlewich Station has been closed since 1960. The salt extraction industry continues on land between the Trent and Mersey Canal and the railway. Alongside the canal there is an area of waste lime beds contained by large bunds. Murgatroyd's Brine Works (a scheduled monument) sits adjacent to the railway line, this being a rare survival of a 'wild' brine pumping plant with typical features of its 19th century operation. Most of the eastern part of Middlewich is occupied by industrial estates comprising metal sheds with red brick offices.



Key landscape characteristics

With a largely Victorian core, the town has substantially increased in size through the introduction of housing developments over the post-war period. The town retains substantial areas of industry and is a local centre for retail, community and leisure activity with many recreational routes, including the canals, providing connections to the surrounding countryside.



- | | | | |
|---|--|--|---|
| <ul style="list-style-type: none"> — Route on surface — LCA boundary — Community area — Route in tunnel | <ul style="list-style-type: none"> 1 Town Wharf 2 The River Wheelock 3 The River Dane 4 Wheelock Street 5 Church of Saint Michaels and All Angels | <ul style="list-style-type: none"> 6 Cledford Hall 7 The Shropshire Union Canal 8 Trent and Mersey Canal 9 River Croco 10 Wardle Lock | <ul style="list-style-type: none"> 11 British Salt 12 Sandbach to Northwich railway 13 Murgatroyd's Brine Works Scheduled Monument |
|---|--|--|---|

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Key landscape value attributes

Key landscape value attributes of the Middlewich LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

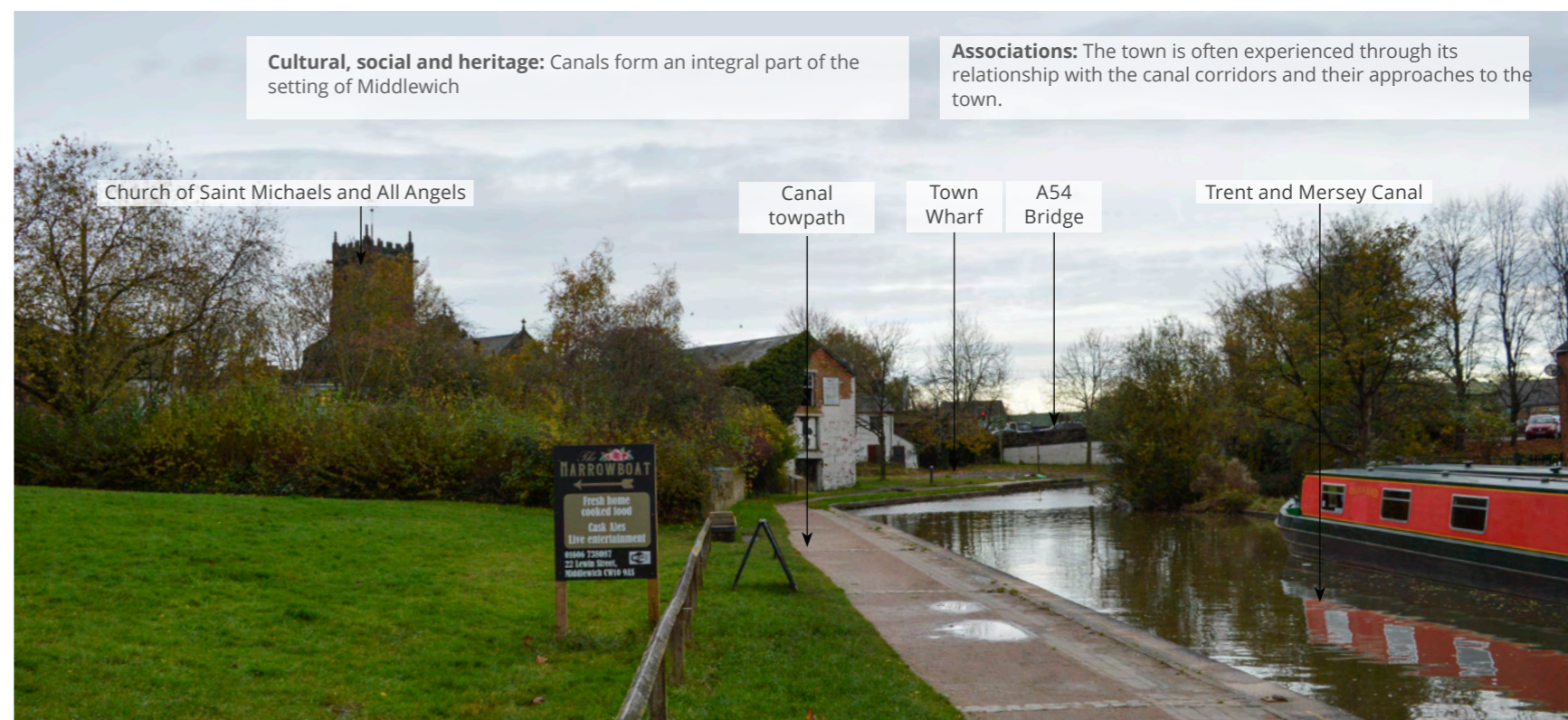
The 19th Century buildings of the town centre feature some unusual architectural details such as colourful tiling, moulded plasterwork and black and white timber. The canal corridors provide a scenic quality and a sense of tranquillity particularly along their more heavily vegetated lengths in contrast with the surrounding urban townscape. Modern housing and industrial estates degrade the overall aesthetic value of the town.



Cultural, social and historic

The town's association with the salt extraction industry is evidenced through historical features such as Murgatroyd's Brine Works and the current production plant which is situated off Cledford Lane. The three canals in the town include Wardle Lock Branch which is considered to be shortest in the UK at 30m in length. The town centre features many listed buildings and several footpaths and cycleways offer valuable recreational opportunities.

Key landscape characteristics susceptible to the AP1 revised scheme



Overall landscape value

The town of Middlewich sits on the plain between the rivers Dane and Wheelock with slight local variation in levels at the River Croco and other smaller tributary watercourses. This is an urban area with limited natural habitat, although the waste lime beds to the south-east have the potential to support grassland species. Most of the town consists of housing areas but with a core of retail services and leisure centred on a main shopping street of largely 19th Century buildings. The town's connections to its Roman past and links to the Industrial Revolution are maintained through features such as a replica Roman theatre and current canal activity, with continuing salt manufacturing and regular cultural festivals.

Features of aesthetic and historic value are scattered within the large areas of housing and industry. The value of this LCA is therefore **medium-low** given the above.

Overall landscape susceptibility

The urban character of the town centre, with its assemblage of buildings of largely Victorian and earlier origins is susceptible to change as a result of the AP1 revised scheme. It retains a sense of historic continuity, derived from the urban canal corridors of the Trent and Mersey Canal and Shropshire Union Canal (Middlewich Branch). However, the level of susceptibility is lessened somewhat by the character associated with the outer parts of the town, comprising areas of industry and modern housing. The landscape has a **medium-low** susceptibility to change resulting from the AP1 revised scheme.

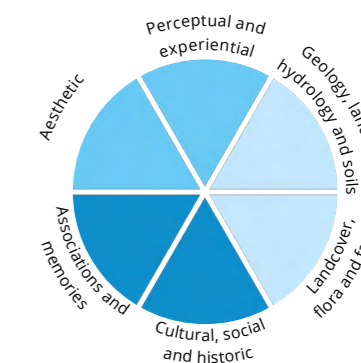
Future baseline

There are no committed developments which will change the baseline.

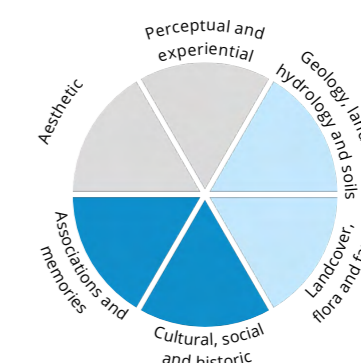
Overall landscape sensitivity

Middlewich has a sense of historic continuity and features of recreational value which are susceptible to the AP1 revised scheme, but when balanced with the presence of existing industry and infrastructure, the resulting sensitivity of this LCA is **medium-low**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

The main ES reported a **minor** adverse (non-significant) effect. This would be the result of construction traffic using the A54 Middlewich Road/Chester Road/St. Michael's Way/Kinderton Street and Holmes Chapel Road and the presence of utility works. However, these changes would not be out of character within the urban character of this LCA. During construction, the AP1 amendment, additional land required for modifications to the A54 Chester Road/A530 Croxton Lane junction (AP1-002-007) will not change the landscape effect on Middlewich LCA as described in the main ES. This is because the AP1 amendment will result in only slight changes to the road layout within the existing highway boundary and effects will be localised. However, a small number of existing, mature, roadside lime trees will be removed as part of the works. Therefore, the AP1 amendment will not give rise to a different effect for Middlewich LCA and will not change the level of significance of the effect reported in the main ES.

Effects will be **minor adverse (non-significant)**.

Operation year 1

The main ES reported a **negligible** (non-significant) effect. This would be due to the original scheme lying beyond the boundaries of Middlewich LCA, resulting in no direct landscape effects for this LCA. Indirect landscape effects would be contained to the west of the LCA by intervening vegetation. At year 1, the AP1 amendment will not change the landscape effect on Middlewich LCA as described in the main ES. This is because the AP1 amendment will result in only minor changes to the road layout within the existing highway boundary and effects will be localised. Roadside trees removed during construction, will be replaced, but will not be sufficiently mature to restore the character of the tree-lined road corridor. Therefore, the AP1 amendment will not give rise to a different effect for Middlewich LCA and will not change the level of significance of the effect reported in the main ES.

Effects will be **negligible (non-significant)**.

Operation year 15

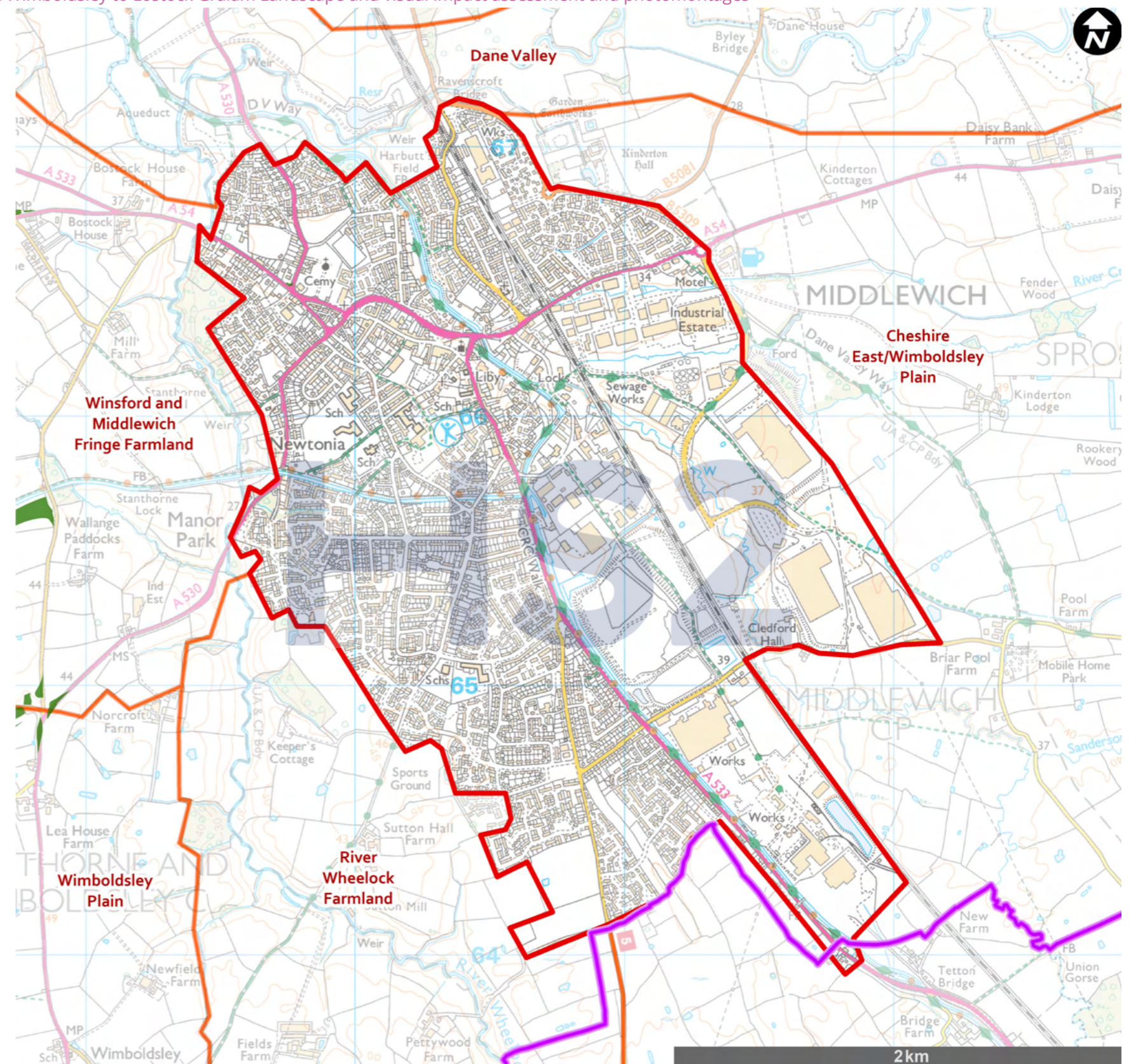
The main ES reported a **negligible** (non-significant) effect. This would be due to the original scheme lying beyond the boundaries of Middlewich LCA, resulting in no direct landscape effects for this LCA. Indirect landscape effects would be contained to the west of the LCA by intervening vegetation and maturing mitigation planting. At year 15, the AP1 amendment will not change the landscape effect on Middlewich LCA as described in the main ES. This is because the changes to the road alignment will be barely perceptible within the existing highway boundary. Roadside trees removed during construction, will have been replaced and will be sufficiently mature to restore the character of the tree-lined road corridor. Therefore, the AP1 amendment will not give rise to a different effect for Middlewich LCA and will not change the level of significance of the effect reported in the main ES.

Effects will be **negligible (non-significant)**.

Operation year 30

The main ES reported a **negligible** (non-significant) effect. This would be due to the original scheme lying beyond the boundaries of Middlewich LCA, resulting in no direct landscape effects for this LCA. Indirect landscape effects would be contained to the west of the LCA by intervening vegetation and maturing mitigation planting. At year 30, the AP1 amendment will not change the landscape effect on Middlewich LCA as described in the main ES. This is because the changes to the road alignment will be barely perceptible within the existing highway boundary. Roadside trees removed during construction, will have been replaced, and will be sufficiently mature to restore the character of the tree-lined road corridor. Therefore, the AP1 amendment will not give rise to a different effect for Middlewich LCA and will not change the level of significance of the effect reported in the main ES.

Effects will be **negligible (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

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Wincham Brook And Holford Lower Wooded Farmland LCA

Landscape character baseline description

This LCA is an area of arable and pastoral land to the north-east of Northwich, characterised by a network of heavily wooded, incised stream courses and areas of woodland on former mossland.

The field pattern within the LCA varies between irregular small and medium-sized fields to larger, more regular enclosure. Most fields are bordered by mature hedgerows, frequently with ponds originating from marl pits. The meandering stream courses of Smoker Brook and Peover Eye flow east to west, converging to form Wincham Brook. Distinctive belts of woodland flank these incised stream courses at Winnington Wood, Smoker Wood and Leonard's Wood, parts of which are ancient and semi-natural woodland. To the south-east, Plumley Lime Beds Site of Special Scientific Interest (SSSI) and Holford Moss incorporate large tracts of woodland with intervening grassland and wetland habitats. A network of local footpaths runs close to these woodlands and to the moated site of Holford Hall, linking the village of Plumley with Lostock Gralam.

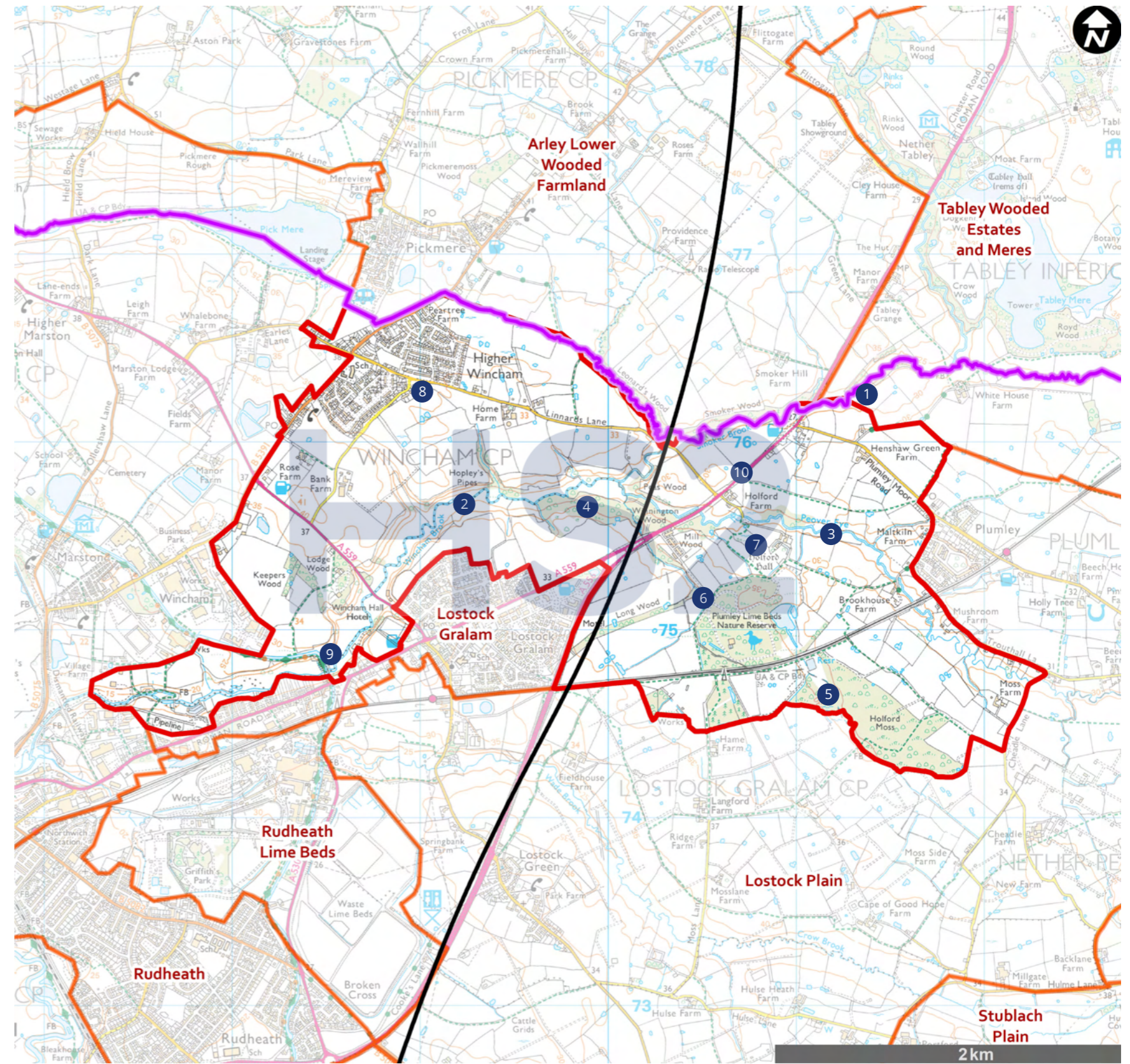
Higher Wincham is located in the north-western part of the LCA and comprises predominantly 20th Century housing, whilst the residential and industrial fringes of Lostock Gralam and Wincham edge the valley of Wincham Brook. The valley is crossed by the Trent and Mersey Canal, the canal being an important recreation corridor and historic asset linking local settlements.

Beyond Higher Wincham, settlement is limited to dispersed farmsteads and clusters of properties alongside local roads. The highly trafficked A556 Chester Road/Shurlach Road and the Mid Cheshire Line railway cross the landscape to the east of Lostock Gralam, both of which are detracting elements in the landscape. However, the density of roadside vegetation and surrounding woodland limits the influence of this linear infrastructure. This LCA is an area of arable and pastoral land to the north-east of Northwich, characterised by a network of heavily wooded, incised stream courses and areas of woodland on former mossland.



Key landscape characteristics

This is a flat, well-wooded landscape which is generally able to accommodate detracting infrastructure elements such as the A556 Shurlach Road, due to the presence of mature, enclosing vegetation. The LCA provides opportunities for recreational use of footpaths and tracks amongst the woodland.



- Route on surface
- LCA boundary
- Community area
- The Cheshire Ring Canal Walk
- 1 Smoker Brook
- 2 Wincham Brook
- 3 Peover Eye
- 4 Winnington Wood
- 5 Holford Moss
- 6 Plumley Lime Beds Nature Reserve
- 7 Holford Hall
- 8 Higher Wincham
- 9 Trent and Mersey Canal
- 10 A556 Chester Road/ Shurlach Road

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Key landscape value attributes

Key landscape value attributes of the Wincham Brook and Holford Lower Wooded Farmland LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

Aesthetic quality varies between large areas of tranquil and remote rural fields and woodlands with scattered farmhouses and ordinary quality modern housing in Higher Wincham village to the north-west. The pattern of woodland limits the influence of the A556 Chester Road, railway and edges of the surrounding villages on the rural landscape.



Cultural, social and historic

There are two scheduled monuments in this area, Plumley Lime Beds Nature Reserve and Holford Hall moated site. Holford Hall, which is also Grade II* listed, is a 17th Century country house. The Trent and Mersey Canal, a designated conservation area, passes through the far west of the LCA with the Cheshire Ring Canal Walk following its towpath.

Key landscape characteristics susceptible to the AP1 revised scheme



Overall landscape value

The landform contrasts between flat plain and steep-sided watercourses which are frequently lined by woodland. There is ancient woodland at Winnington Wood, Smoker Wood and Leonard's Wood and woodland with grassland and wetland habitats at Holford Moss and Plumley Lime Beds SSSI.

The area generally feels tranquil with an intimate quality to its many woodlands and small watercourses.

The landscape has many areas of scenic quality and historic value which are only partially influenced by development or infrastructure. The value of this LCA is therefore **medium** given the above.

Overall landscape susceptibility

The narrowly incised watercourses and the prevalence of long-established woodland, in addition to mossland areas designated for their biodiversity value are all vulnerable to change. The landscape currently accommodates existing major road and rail infrastructure, primarily due to areas of woodland containing their influence on the surrounding landscape. The landscape therefore has a **medium** susceptibility to change resulting from the AP1 revised scheme.

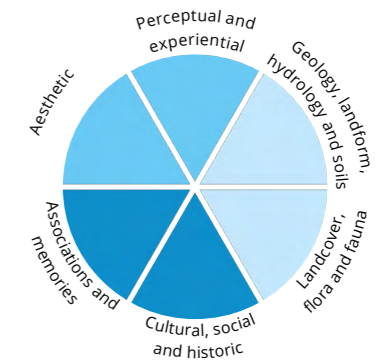
Future baseline

There are no committed developments which will change the baseline.

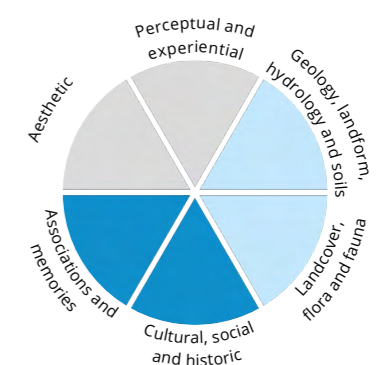
Overall landscape sensitivity

The scenic value and sense of historic continuity of this area is balanced with the presence of encroaching urban development and linear infrastructure. When considered with the susceptibility of the landscape, including its historic and natural features, this results in the sensitivity of this LCA being **medium**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

The main ES reported a **moderate** adverse (significant) effect. This would be the result of large-scale construction activity in association with Smoker Brook viaduct over the A556 Chester Road/Shurlach Road and Lostock Gralam north embankment, the presence of two satellite compounds and the removal of mature tree cover including ancient woodland. During construction, the AP1 amendment, additional land permanently required for the realignment and extension of Smoker Brook viaduct at the A556 Shurlach Road and Winnington Wood (AP1-002-012) will not change the landscape effect for the Wincham Brook and Holford Lower Wooded Farmland LCA. This is because the AP1 amendment will result in only a slight modification to the extent of land required for construction of the AP1 revised scheme. The small-scale of the AP1 amendment will not noticeably change effects on landscape character when considered in the context of the large-scale construction works and wide area affected by the original scheme. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **moderate adverse (significant)**.

Operation year 1

The main ES reported a **moderate** adverse (significant) effect. This would be the result of the introduction of train movements and large-scale infrastructure including Smokers Brook viaduct, into the rural landscape and the loss of trees and woodland during construction. At year 1, the AP1 amendment will not change the landscape effect for the Wincham Brook and Holford Lower Wooded Farmland LCA. This is because the overall area of woodland removed during construction from Winnington Wood, Leonard's Wood and Smoker Wood will remain largely the same as in the original scheme. The small-scale of the AP1 amendment will not noticeably change the effects on landscape character when considered in the context of the large-scale elements and wide area affected by the original scheme. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **moderate adverse (significant)**.

Operation year 15

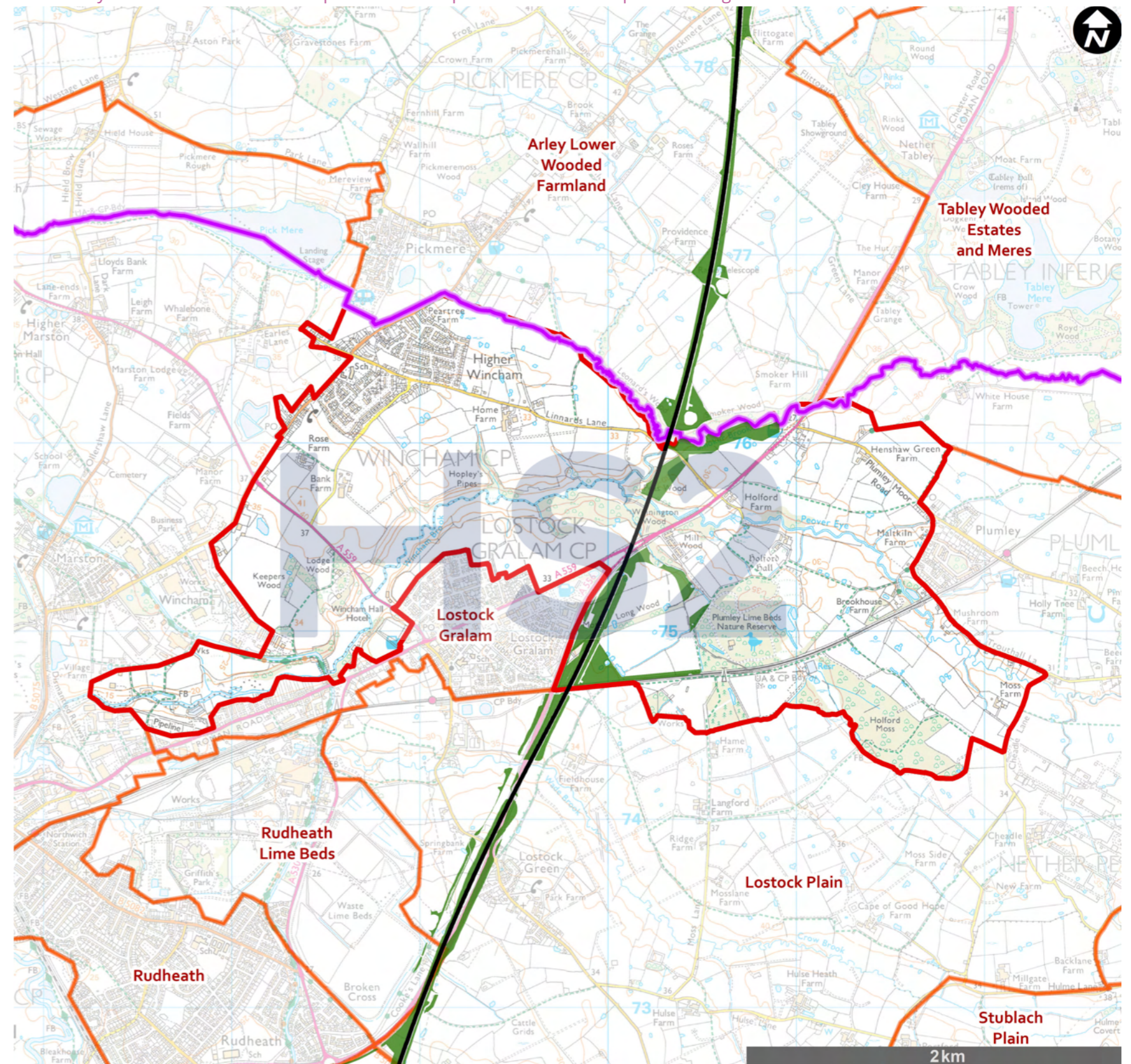
The main ES reported a **minor** adverse (non-significant) effect. This would be the result of the partial integration of the original scheme into the wooded, rural landscape of the LCA by maturing mitigation planting. At year 15, the AP1 amendment will not change the landscape effect for the Wincham Brook and Holford Lower Wooded Farmland LCA. This is because replacement woodland planting will be sufficiently mature to partially restore the character of woodland lost during construction. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **minor adverse (non-significant)**.

Operation year 30

The main ES reported a **minor** adverse (non-significant) effect. This would be the result of further integration of the original scheme into the wooded, rural landscape of the LCA by maturing mitigation planting. At year 30, the AP1 amendment will not change the landscape effect for the Wincham Brook and Holford Lower Wooded Farmland LCA. This is because replacement woodland planting will be sufficiently mature to restore the character of woodland lost during construction. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **minor adverse (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

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Holmes Chapel LCA

Landscape character baseline description

A large Cheshire village on the A50 Knutsford Road/London Road/A535 Macclesfield Road and A54 Chester Road. It is situated east of Middlewich and south of the River Dane.

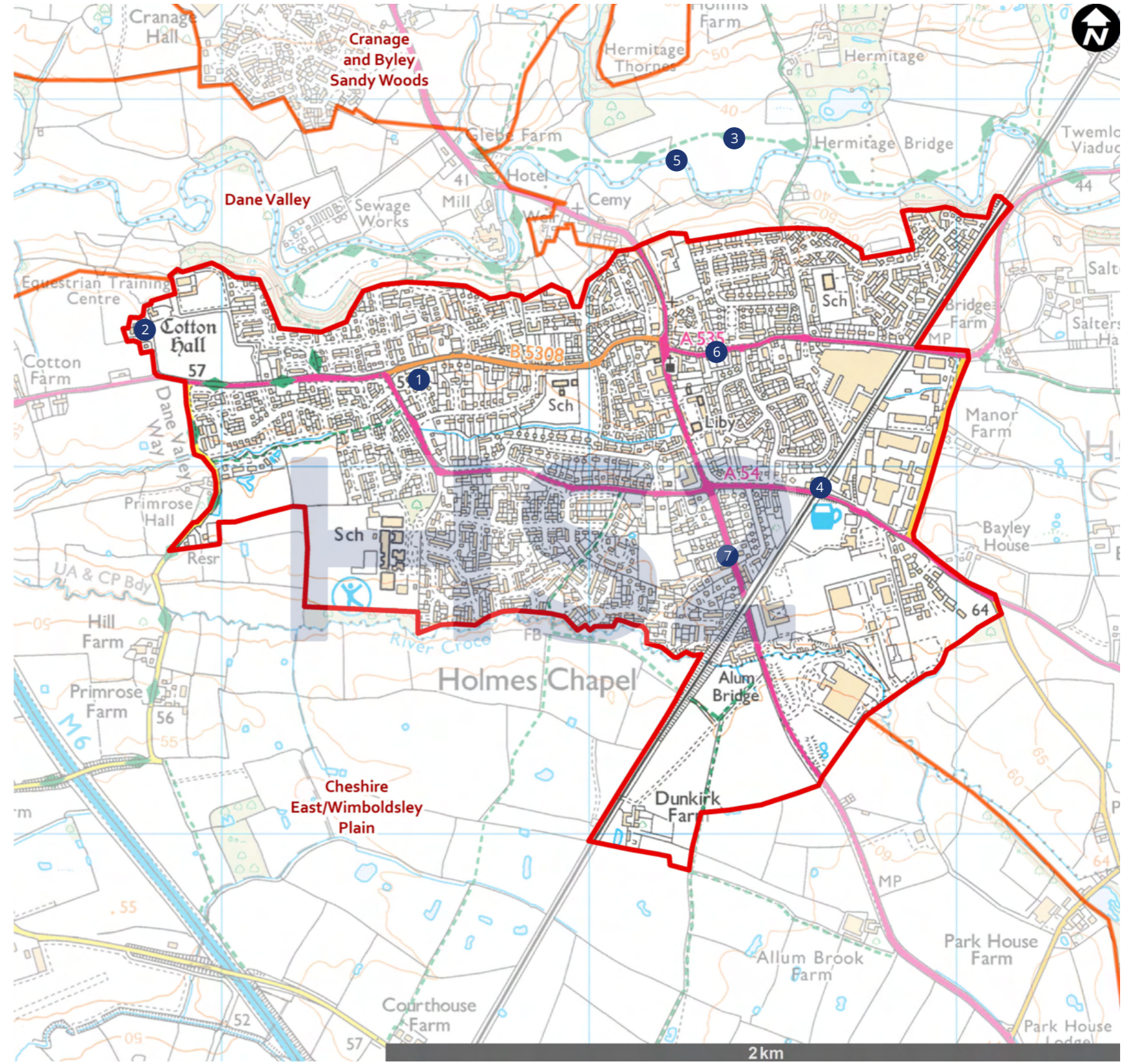
Located within the Cheshire Plain landscape, the village has grown around its historic core which is partly within the Holmes Chapel Conservation Area. The Grade I listed St Luke's Church, which dates from around 1430, is a prominent feature at the centre of the settlement. Close by are Georgian and Victorian buildings, which house shops and other commercial uses, and Victorian and Edwardian terraced housing. The Grade II* listed Cotton Hall, a late 15th Century farmhouse, sits in its grounds at the western end of the LCA. The village has expanded in recent times with small to medium sized housing estates, typically laid out along feeder roads and cul-de-sacs. The housing tends to be larger detached and semi-detached properties with front and back gardens.

The main roads leading into the village are often lined with mature trees but the more modern estates are mainly planted with smaller ornamental trees. Remnants of hedgerows and mature trees can be found on the periphery of the estates, where there is a distinct change from residential development to open countryside. Public open space is generally linked to schools but there are paths leading to the Dane Valley and the Dane Valley Way trail from the northern and western boundaries of the settlement and to the Cheshire Countryside from the southern boundary. The Crewe to Manchester Line passes through the village and divides the LCA into a predominantly residential area to the west and a light industrial area to the east. The railway is well integrated into the townscape by mature lineside planting. The busy main roads and railway line reduce tranquillity locally but it is higher in residential areas and along the River Dane.



Key landscape characteristics

The village sits slightly above the Dane Valley, within the flat landform of the Cheshire Plain. It is a compact settlement, largely contained by the River Dane, the Crewe to Manchester Line and the River Croco to the south. A network of PRoW and paths allow access to the Dane Valley and the Cheshire Countryside. The roads leading into the village are tree-lined but the centre and 20th Century residential areas have a more open character.



- Route on surface
- LCA boundary
- 1 St Luke's Church
- 2 Cotton Hall
- 3 Dane Valley Way trail
- 4 Holmes Chapel railway station
- 5 River Dane
- 6 A535 Macclesfield Road
- 7 A54 Chester Road

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Key landscape value attributes

Key landscape value attributes of the Holmes Chapel LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

The Grade I listed St Luke's Church at the heart of the village, along with other historic buildings, contributes to the historic character of the village. Trees lining the main approach roads, the Dane Valley to the north and the countryside to the south provide an attractive and rural context to the village.



Cultural, social and historic

A settlement has existed here since the medieval era. It grew up around the junction of the two strategic roads, the A50 Knutsford Road/London Road/A535 Macclesfield Road and the A54 Chester Road. There are several schools and local shops and services within Holmes Chapel LCA, serving the village and the outlying areas and making Holmes Chapel a local hub of activity.

Key landscape characteristics susceptible to the AP1 revised scheme



Overall landscape value

The village of Holmes Chapel is a compact settlement close to the Dane Valley. It has extensive areas of modern residential development, but retains some historic character centred around St Luke's Church and the Holmes Chapel Conservation Area. The busy through roads and light industrial development at the periphery of the LCA are detracting elements. The value of this LCA is therefore **medium**.

Overall landscape susceptibility

The historic buildings, conservation area, tree-lined approaches to the village and the rural setting of the LCA within the wider landscape are susceptible to change. The presence of light industrial and infrastructure elements lower susceptibility. The landscape therefore has a **medium** susceptibility to change resulting from the AP1 revised scheme.

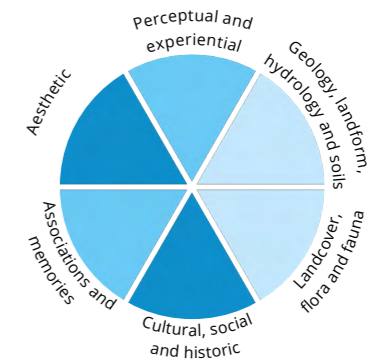
Future baseline

There are no committed developments which will change the baseline.

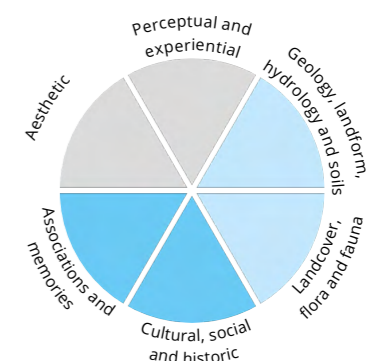
Overall landscape sensitivity

Holmes Chapel has a number of historic buildings at its core and is a lively local centre for the area but the majority of the more recent development lacks distinctiveness. Tranquillity is reduced by the railway and busy roads that pass through the village. There are good connections between the settlement and the countryside to the north and south and the Dane Valley Way which contribute to the recreational value of the area. The overall sensitivity of this of this LCA is **medium**.

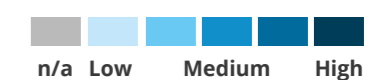
Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

This new LCA is located in an area which would not be affected by the original scheme. During construction, the AP1 amendment, additional land permanently required for modifications to the A54 Middlewich Road and Chester Road junction (AP1-002-009) will result in modifications to the road junction and carriageway widening. Several roadside trees, that contribute to the landscape character of this main route into the village of Holmes Chapel, will be removed. The AP1 amendment will introduce construction activity and construction vehicle movements across a small proportion of the LCA. Landscape effects will be localised and in keeping with the character of the existing highway. The magnitude of change will be **low**.

Effects will be **minor adverse (non-significant)**.

Operation year 1

At year 1, the AP1 amendment will result in changes to the existing road junction, in a localised area of the LCA. Roadside trees removed during construction will have been replaced but will not be sufficiently mature to reinstate the tree-lined character of the existing road corridor. The scale of the change will be largely inconspicuous in the context of the wider LCA. The magnitude of change will be **low**.

Effects will be **minor adverse (non-significant)**.

Operation year 15

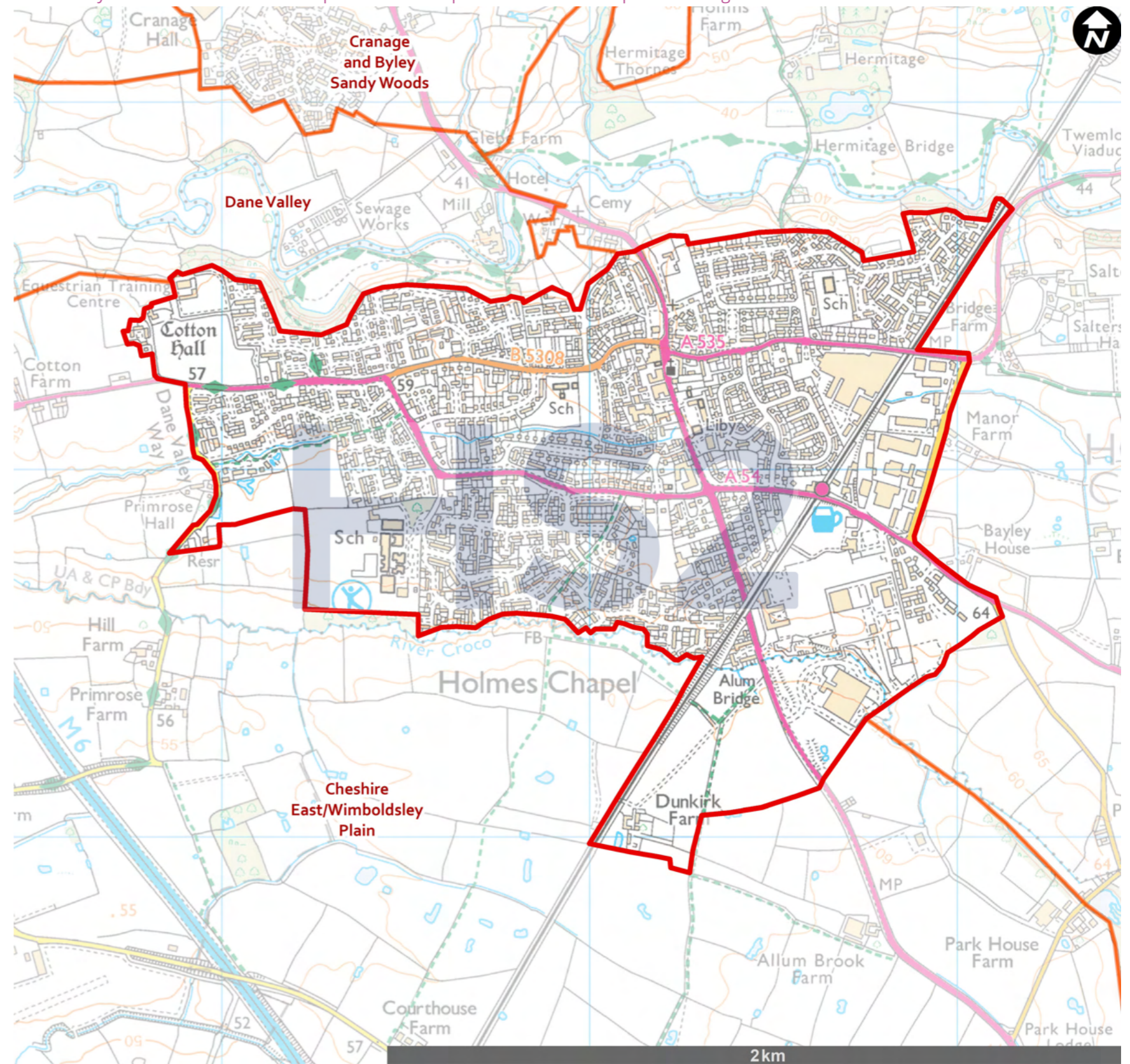
At year 15, maturing roadside trees, that replaced those lost during construction, will partially restore the tree-lined character of the existing road corridor. The magnitude of change will reduce to **negligible**.

Effects will be **negligible (non-significant)**.

Operation year 30

At year 30, mature roadside trees, that replaced those lost during construction, will largely restore the tree-lined character of the existing road corridor. The magnitude of change will remain **negligible**.

Effects will be **negligible (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

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Winsford and Middlewich Fringe Farmland LCA

Landscape character baseline description

This LCA is a gently undulating agricultural landscape between the shallow valleys of the rivers Weaver and Wheelock, separating the urban fringes of Winsford to the west and Middlewich to the east.

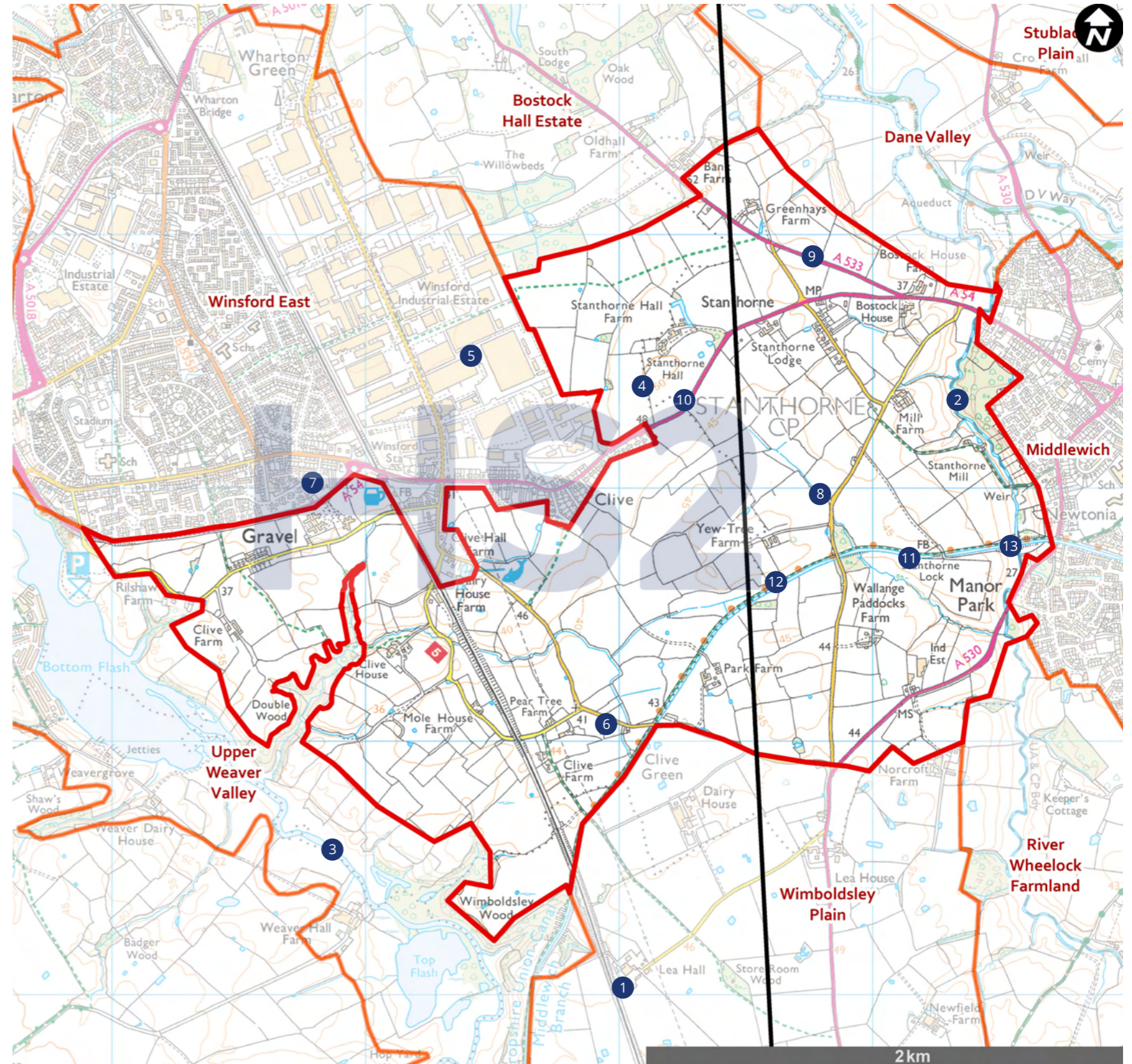
The area is characterised by an historic pattern of small to medium sized irregular fields, most of which are bordered by well-managed hedges incorporating mature trees. There are also frequent individual mature field trees. The landform is gently undulating between converging local watercourses. Along these watercourses, mature vegetation contributes to a sense of enclosure and forms visual buffers to the nearby residential and industrial edges of Middlewich and Winsford.

Beyond the towns, the settlement pattern is one of dispersed large individual farmsteads and houses including the locally prominent Stanthorne Hall. Despite the proximity of Winsford and Middlewich, the landscape retains a largely rural character, although the eastern boundary of Winsford Industrial Estate is a noticeable detractor. The West Coast Main Line severs the landscape between Clive Green and Winsford, while the A530 Nantwich Road, A533 Northwich Road/Bostock Road and A54 Middlewich Road radiate across the landscape from Middlewich. The Shropshire Union Canal (Middlewich Branch) is a key feature of the LCA, affording a sense of historic continuity and incorporating several Grade II listed structures, including the aqueduct over the River Wheelock and Stanthorne Lock.



Key landscape characteristics

The combination of ancient field patterns, mature field boundaries and riparian woodland creates a generally peaceful and rural landscape, which belies its proximity to the towns of Winsford and Middlewich.



- Route on surface
 - LCA boundary
 - 1 WCML
 - 2 The River Wheelock
 - 3 The River Weaver
 - 4 Stanthorne Hall
 - 5 Winsford Industrial Estate
 - 6 Clive Green
 - 7 Winsford
 - 8 A530 Croxton Lane
 - 9 A533 Bostock Road
 - 10 A54 Middlewich Road
 - 11 Stanthorne Lock
 - 12 The Shropshire Union Canal (Middlewich Branch)
 - 13 Aqueduct
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Key landscape value attributes

Key landscape value attributes of the Winsford and Middlewich Fringe Farmland LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

The irregular field pattern in combination with frequent hedges, trees and riparian vegetation creates a sense of local distinctiveness, despite the proximity of Middlewich and Winsford. The urban fringes of both towns are generally well integrated into their landscape setting by vegetation. However, the abrupt boundary of Winsford Industrial Estate is a notable exception. The rail and power lines that cross the landscape between Winsford and Clive Green are also detracting features. The Shropshire Union Canal (Middlewich Branch) has a scenic quality associated with the farmland to the south of the settlements.



Cultural, social and historic

The Shropshire Union Canal (Middlewich Branch) has several listed historic structures and is a valued recreation corridor linking town and country for boating, walking and cycling. Substantial historic properties in the area, such as Stanthorne Hall and Park Farmhouse, are listed and are often prominent features of the landscape, adding to the area's sense of history.

Key landscape characteristics susceptible to the AP1 revised scheme



Overall landscape value

The area forms a buffer landscape between the towns of Middlewich and Winsford. It is accessible recreationally via the Shropshire Union Canal (Middlewich Branch) corridor, and via a connecting network of footpaths and cycle routes. It has some sense of tranquillity despite its proximity to the towns, although this is interrupted by overhead power lines and transport routes including the WCML, the A530 Nantwich Road and A54 Middlewich Road, along with the industrial edge of Winsford. The tight-knit field pattern and high frequency of hedges and mature trees, in particular between Clive and Clive Green, contrast with the more intensive agricultural landscape to the south. In the former, there is a stronger sense of old lanes and historic settlements. The Shropshire Union Canal (Middlewich Branch), locally prominent farms and estate properties all add to the sense of historic continuity. The value of this LCA is **medium**.

Overall landscape susceptibility

The LCA forms a buffer landscape between the towns of Winsford and Middlewich. It retains a sense of historic continuity, derived from the Shropshire Union Canal (Middlewich Branch), local settlements and field patterns, all of which are susceptible to the type of development proposed. However, the landscape is influenced by existing transport and energy infrastructure including the WCML and major roads. It has a **medium** susceptibility to change resulting from the AP1 revised scheme.

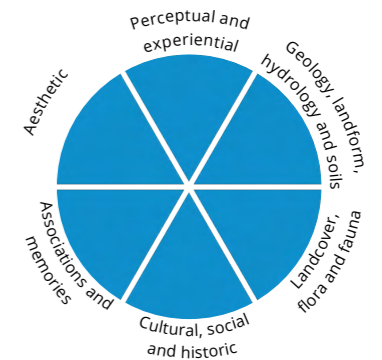
Future baseline

There are no committed developments which will change the baseline.

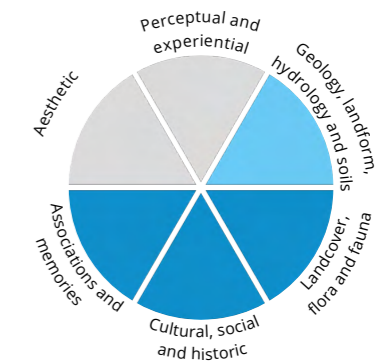
Overall landscape sensitivity

The value of this area as a largely intact agricultural landscape between the towns of Middlewich and Winsford, in combination with the cultural value, social value and susceptibility of its defining historic landscape features results in the sensitivity of this LCA being **medium**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

The main ES reported a **major** adverse (significant) effect. This would be as a result of the introduction of large-scale construction works including five satellite compounds, in association with the realignment of the A54 Middlewich Road; the excavation of MA02 Borrow Pit C; and the realignment of Clive Green Lane. During construction, the AP1 amendment, additional land permanently required for the provision of a shared use cycle and pedestrian path at Clive Green Lane (AP1-002-004) will not change landscape effects for the Winsford and Middlewich Fringe Farmland LCA. This is because construction of the proposed cycle/pedestrian path, ramped access to the canal and associated earthworks, will be localised within a small proportion of the LCA and will be largely inconspicuous when considered in the context of the large-scale construction works for the original scheme. The AP1 amendment will therefore not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **major adverse (significant)**.

Operation year 1

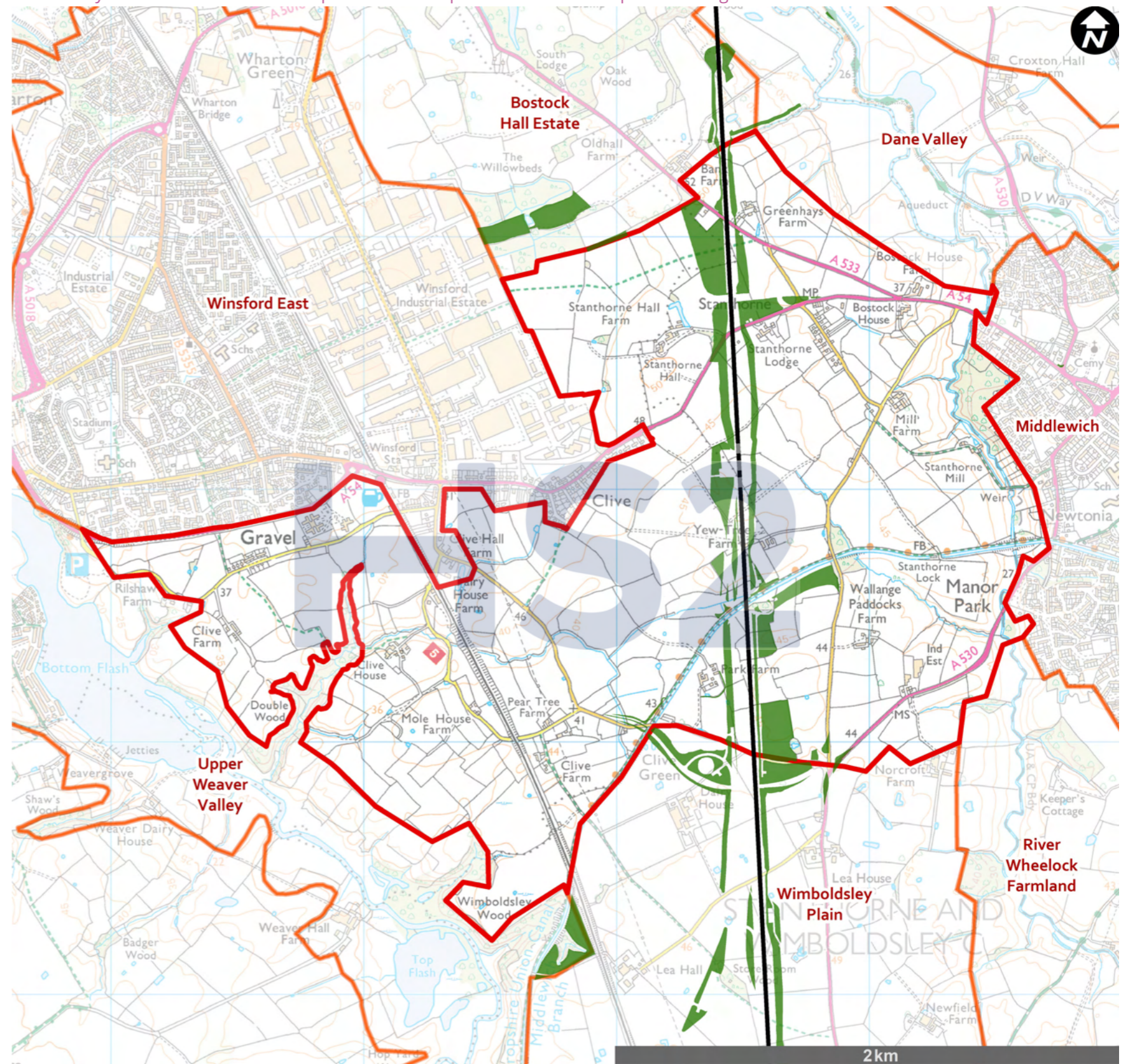
The main ES reported a **moderate** adverse (significant) effect. This would be as a result of the introduction of large-scale infrastructure including the realigned Clive Green Lane, A54 Middlewich Road and A533 Northwich Road/Bostock Road, Shropshire Union Canal viaducts No. 1-3 and train movements, into the predominantly rural landscape. The loss of vegetation removed during construction would be a noticeable change to one of the key characteristics of the landscape. At year 1, the AP1 amendment will not change landscape effects for the Winsford and Middlewich Fringe Farmland LCA. This is because the proposed cycle/pedestrian path, ramped access to the canal and associated earthworks, will occupy a small proportion of the LCA and will be largely inconspicuous in the context of the large-scale infrastructure introduced in the original scheme. Landscape mitigation planting along the ramped access from the canal to re-instate trees removed during construction, will not be sufficiently mature to integrate the AP1 amendment into its landscape context. The AP1 amendment will therefore not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **moderate adverse (significant)**.

Operation year 15

The main ES reported a **moderate** adverse (significant) effect. This would be as a result of maturing mitigation planting and landscape earthworks which would largely contain landscape effects and integrate the original scheme into its landscape context. However, the original scheme would still represent a noticeable change to the rural character of the landscape. At year 15, the AP1 amendment will not change landscape effects for the Winsford and Middlewich Fringe Farmland LCA. This is because maturing landscape mitigation planting along the ramped access from the canal to re-instate trees removed during construction, will partially integrate the AP1 amendment into its landscape context. The AP1 amendment will therefore not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **moderate adverse (significant)**.



Operation year 30

The main ES reported a **moderate** adverse (significant) effect. This would be as a result of the greater maturity of landscape mitigation planting further integrating the original scheme into its landscape context. Taller elements such as the Shropshire Union Canal viaducts Nos 1-3 remained uncharacteristic structures within the rural landscape. At year 30, the AP1 amendment will not change landscape effects for the Winsford and Middlewich Fringe Farmland LCA. This is because, the greater maturity of landscape mitigation planting along the ramped access from the canal will further integrate the AP1 amendment into its landscape context. The AP1 amendment will therefore not give rise to a different effect for the Winsford and Middlewich Fringe Farmland LCA and will not change the level of significance of the effect reported in the main ES.

Effects will be **moderate adverse (significant)**.

Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

Lostock Plain LCA

Landscape character baseline description

This LCA is an area of predominantly flat agricultural land which was formerly heath and is now characterised in part by the extraction of brine. It is bordered by the industrial and residential fringes of Northwich to the west, the Stublach Plain to the south and the wooded area of Marthall to its north.

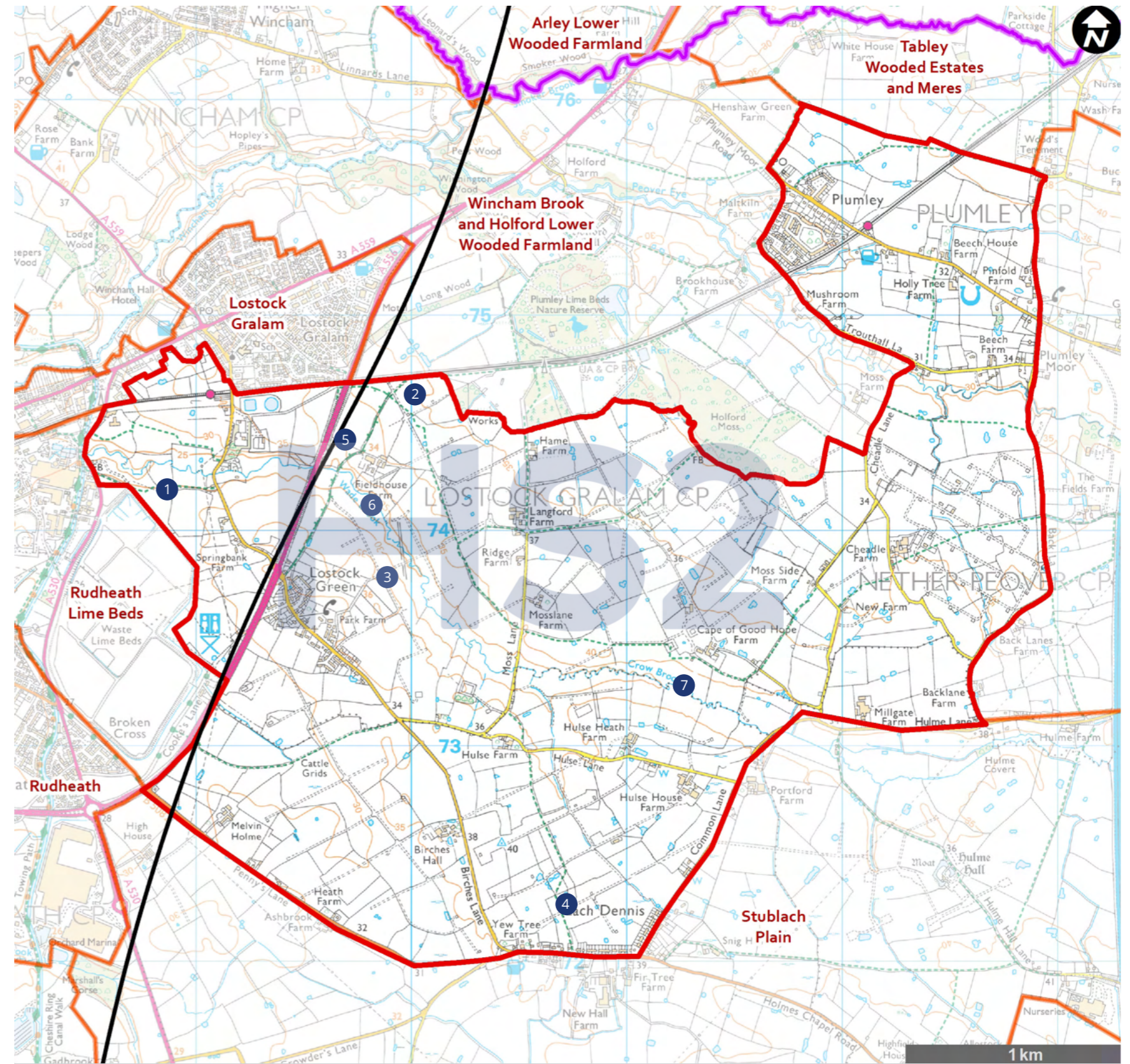
The field pattern comprises a network of small and irregular medieval fields surrounding the compact linear villages of Lostock Green and Lach Dennis, with a pattern of larger sized and more regular enclosure beyond. Agricultural use is predominantly pasture, with a large distribution of field ponds where marl was previously extracted for improvement of the land. Crow Brook and Wade Brook flow east-west across the area, interrupting an otherwise level terrain. A network of drainage ditches trace field boundaries that are lined by largely intact and mature hedgerows. Woodland cover is sparse, consisting of isolated small copses and hedgerow trees. Individual farmsteads, typically of red brickwork construction, punctuate the landscape of the open plain. There is a network of local footpaths, mainly following farm tracks and field boundaries.

This is a major brine extraction area which is also utilised for underground gas storage. These activities create an above ground, visible infrastructure of wellheads, fenced compounds and connecting tracks, evident throughout the landscape. The busy A556 Shurlach Road passes east of Northwich, its mature flanking vegetation creating a buffer to the town's industrial and residential fringe. Overhead power lines are evident to the west of Lostock Green and in the open landscape to the south.



Key landscape characteristics

A level agricultural landscape that is interrupted by relatively unobtrusive yet widespread industrial activity. A landscape of open character with a cohesive building vernacular.



- Route on surface
- LCA boundary
- The Cheshire Ring Canal Walk
- Community area
- 1 Industrial Stublach Plain
- 2 Mid-Cheshire Line railway
- 3 Lostock Green
- 4 Lach Dennis
- 5 A556 Shurlach Road
- 6 Wade Brook
- 7 Crow Brook

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Key landscape value attributes

Key landscape value attributes of the Lostock Plain LCA are set out below, interpreting landscape baseline information on the previous page and drawing on findings from field surveys.



Aesthetic

A shallow agricultural landscape with an open aspect, composed of mainly small to medium scale fields bordered by maintained hedges with infrequent field trees and low woodland cover. Traditional red brick farmhouses punctuate the landscape, while industrial installations including overhead power lines, well-heads, fenced compounds and tracks are detracting features.



Cultural, social and historic

Lostock Green forms the focus of built development within the LCA, whilst within the open countryside a number of the red brick farmsteads throughout the area are Grade II listed, reflecting a historic agricultural use of the land and providing some sense of historic continuity. There are several local footpaths following farm tracks and field boundaries, while the area is primarily a working agricultural landscape with limited facility for recreation.

Key landscape characteristics susceptible to the AP1 revised scheme



Landcover, flora and fauna: The landscape forms a continuation of the wider Cheshire Plain, although more visibly influenced by brine extraction and gas storage installations.

Geology and landform: The landscape is a continuation of the Cheshire Plain, largely flat with some localised variation of landform towards stream courses.

Overall landscape value

The landscape is typical of the wider Cheshire Plain, being predominantly agricultural and of level terrain with some localised variation in landform towards water courses and an abundance of field ponds. Tree cover is sparse, although there are some belts of riparian vegetation along stream courses.

The aesthetic quality of the rural landscape is interrupted by numerous discrete but widespread industrial installations, mainly relating to brine extraction. To the west of the area, the tranquillity of the landscape is influenced by the presence of overhead power lines, the A556 Shurlach Road and the perception of the industrial skyline and residential fringes of Northwich. There is a sense of historic continuity, although there is little in terms of definitive character. The value of this LCA is therefore **medium-low**.

Overall landscape susceptibility

The shallow terrain and sparsity of woodland cover create an open landscape character but one where the frequency of boundary hedges, hedge and field trees in combination prevent longer distance views and provide visual containment, limiting the influence of development on the wider landscape. The industrial uses of the land for brine extraction, although discrete, do still impact upon the predominantly agricultural character of the landscape. It therefore has a **medium-low** susceptibility to change resulting from the AP1 revised scheme.

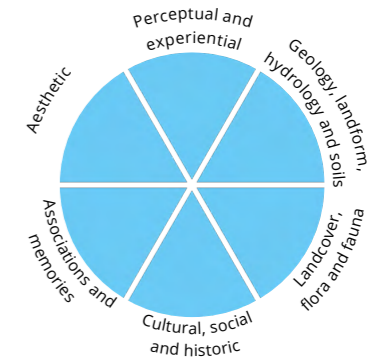
Future baseline

There are no committed developments which will change the baseline.

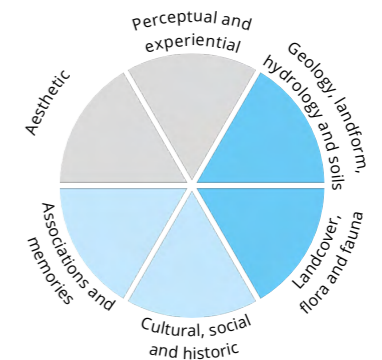
Overall landscape sensitivity

The industrial influence on the landscape, in combination with the inherent landscape characteristics typically associated with the wider Cheshire Plain result in the sensitivity of this LCA being **medium-low**.

Value



Susceptibility



Value and susceptibility key



Magnitude of change and level of effect

Construction

The main ES reported a **moderate** adverse (significant) effect. This would be as a result of, the introduction of large-scale construction works in association with the realignment of the A556 Shurlach Road, construction of Gad Brook viaduct, Wade Brook viaduct and Lostock Gralam viaduct into the predominantly flat, agricultural landscape. During construction, the AP1 amendment, additional land permanently required for the underground diversion of a 11kV Scottish Power Energy Network overhead line at Birches Lane (AP1-002-011), will not change landscape effects for the Lostock Plain LCA. This is because although the rerouting of an underground cable diversion will result in the loss of vegetation along field boundaries and watercourses, the AP1 amendment will occupy a small proportion of the LCA and be largely inconspicuous in the context of the construction of the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **moderate adverse (significant)**.

Operation year 1

The main ES reported a **minor** adverse (non-significant) effect. This would be as a result of the introduction train movements, viaducts, embankments and road realignments into the predominantly flat, agricultural landscape. These new infrastructure elements changed the setting of settlements and buildings in proximity to the original scheme. New earthworks reinforced existing transport corridors. At year 1, the AP1 amendment will not change landscape effects for the Lostock Plain LCA. This is because the AP1 amendment will occupy a small proportion of the LCA and will be barely perceptible in the context of wider changes to landscape character introduced by the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **minor adverse (non-significant)**.

Operation year 15

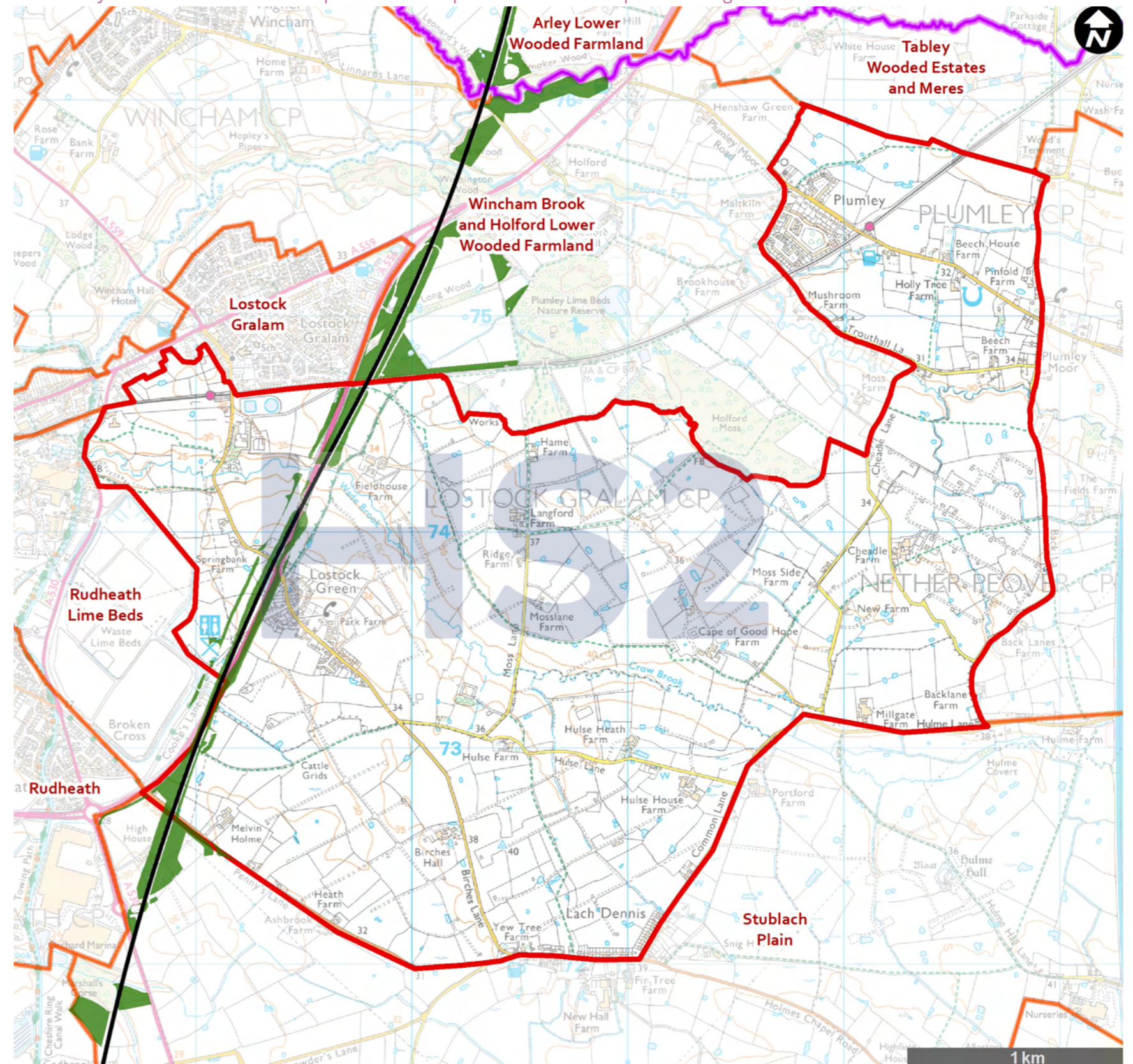
The main ES reported a **minor** adverse (non-significant) effect. This would be as a result of maturing mitigation planting restoring the landscape pattern and integrating the original scheme into its landscape context. At year 15, the AP1 amendment will not change landscape effects for the Lostock Plain LCA. This is because vegetation replacing that which would be lost in construction, will be sufficiently mature to partially restore the character of field boundaries and watercourses. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **minor adverse (non-significant)**.

Operation year 30

The main ES reported a **negligible** (non-significant). This would be as a result of maturing mitigation planting which would integrate the original scheme into the wider landscape. At year 30, the AP1 amendment will not change landscape effects for the Lostock Plain LCA. This is because vegetation replacing that removed in construction, will be sufficiently mature to restore the character of field boundaries and watercourses. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect reported in the main ES.

Effects will be **negligible (non-significant)**.



Cumulative assessment (construction and operation)

Construction: There are no developments which will result in cumulative construction effects.

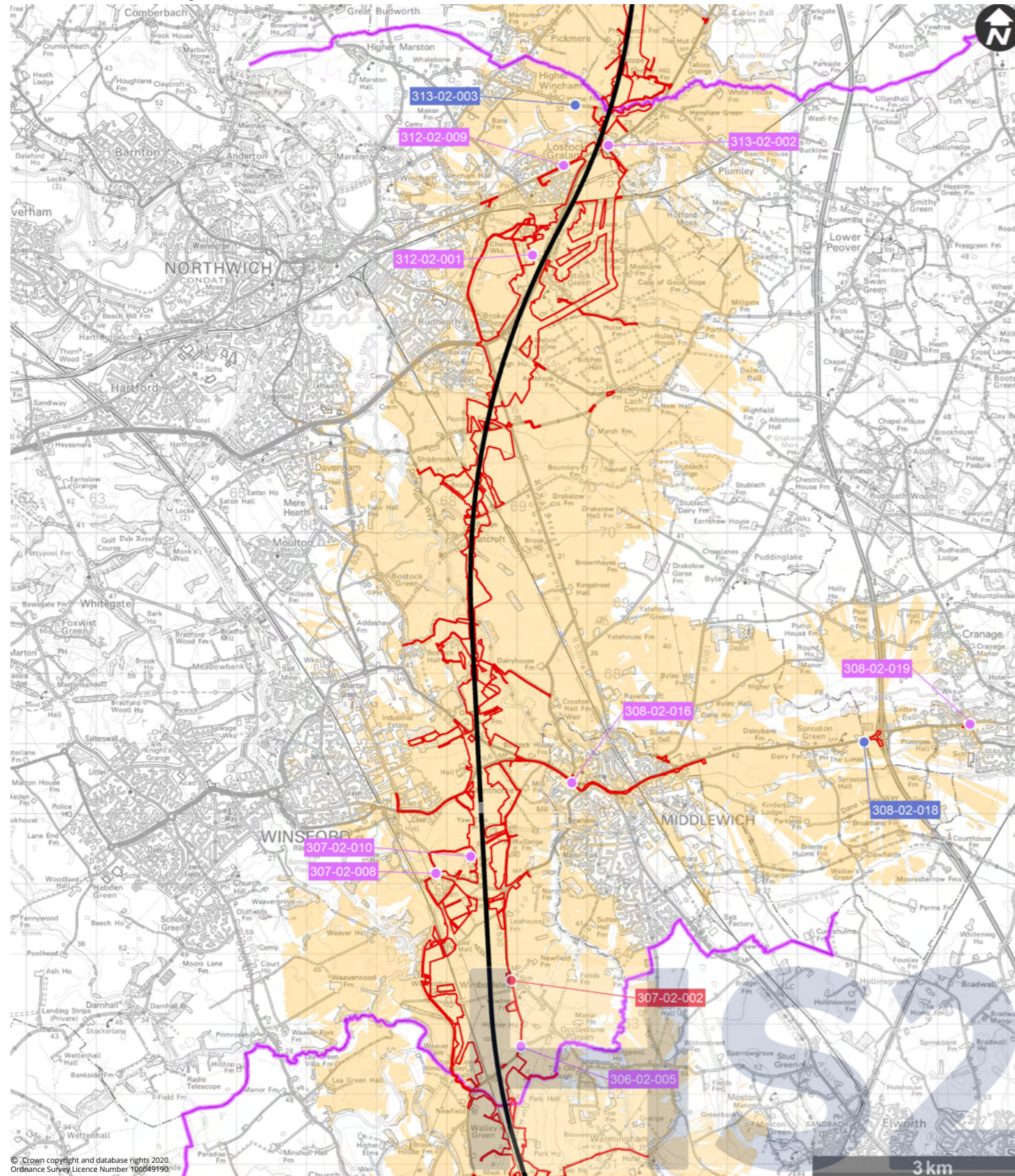
Operation: There are no developments which will result in cumulative operation effects.

No cumulative effects during construction and operation.

4.2 Visual assessment

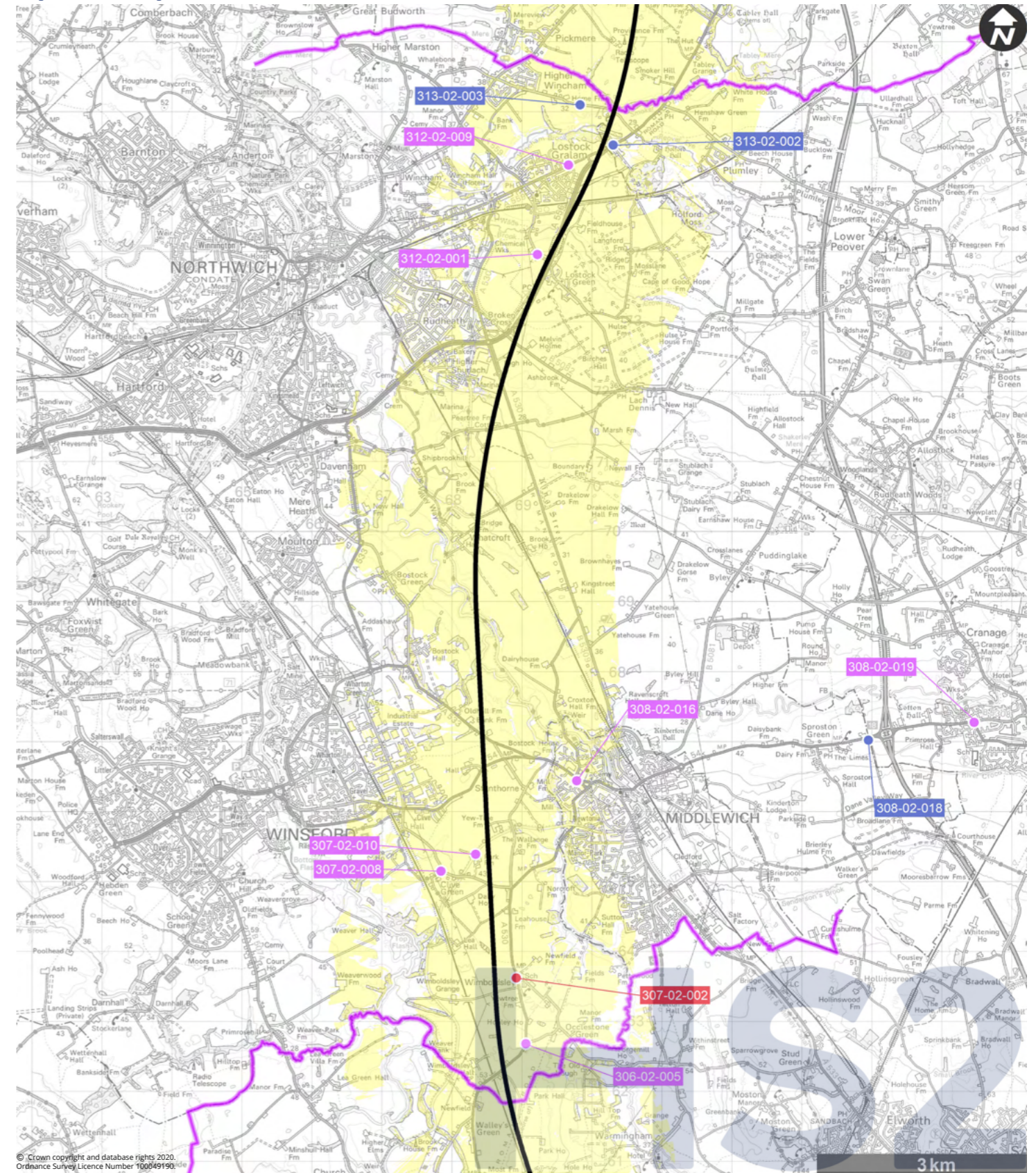
Overview of viewpoints and photomontages in the community area affected by AP1 amendments

Construction phase



- Route in tunnel
- Route on surface
- Community area boundary
- Non significantly affected viewpoint
- Significantly affected viewpoint
- Non significantly affected photomontage
- Significantly affected photomontage
- Land potentially required during construction
- ZTV construction

Operation phase



- Route in tunnel
- Route on surface
- Community area boundary
- Non significantly affected viewpoint
- Significantly affected viewpoint
- Non significantly affected photomontage
- Significantly affected photomontage
- ZTV operation year 1

Viewpoint 306-02-005: view west from Bellsmithy, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in properties bordering the A530 Nantwich Road at Hopley House, Bellsmithy and Yewtree Farm, Manor Cottage, users of Footpath Wimboldsley 3/1, Bridleway Wimboldsley 2/1 and road users of A530 Nantwich Road.

Winter view (baseline)

Date taken: 13/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 07/09/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	369048, 362954	
Value of the viewpoint:	This viewpoint has a medium value, its composition being representative and typical of the open and predominantly agricultural to the south of Middlewich, with the A530 Nantwich Road a noticeable component of the view.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents and footpath users have a strong interest in their visual environment and have a high susceptibility to visual change arising from the construction and operation of the AP1 revised scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.	High



Viewpoint 306-02-005: view west from Bellsmithy, A530 Nantwich Road

Visual baseline description

Winter	The view looks west across the A530 Nantwich Road, with a near distance comprising managed roadside and field boundary hedges with infrequent, isolated roadside trees. Views from properties bordering the A530 Nantwich Road and from footpaths are typically open and unrestricted, although those at Yewtree House and Yewtree Farm to the north are enclosed by garden vegetation. In the middle-distance are large arable fields bordered by managed hedgerows with infrequent trees and occasional small isolated copses; with overhead power lines visible against the skyline. The view extends unrestricted to a distant far distance of field boundary vegetation and woodland belts bordering the River Weaver. Overhead line equipment and train movements along the WCML is visible against this backdrop but does not break the skyline. The rooflines of Wimboldsley Hall and Wimboldsley Grange are both visible in the distant far distance.
Summer	In summer the extent of views does not substantially change, although summer foliage on tree canopies in the vicinity of Yewtree House/Yewtree Farm and Hopley House, heavily filters views west from these properties. The WCML and power lines remain evident across much of the view. Individual field and hedgerow trees appear more prominent and distinctive.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties. There is a far distance skyglow from the town of Winsford approximately 3km to the north-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of large-scale construction works associated with the Crewe North RSD, Walley's Green embankment, its associated landscape earthworks and landscape mitigation planting and the A530 Nantwich Road satellite compound, in the near and middle-distance and the proximity of the borrow pit including Borrow Pit A satellite compound. The construction works were visible across the majority of the view and out of character within the open agricultural landscape. During construction, the AP1 amendment, additional land permanently required for the provision of landscape mitigation planting at Wimboldsley (AP1-002-002), will not change the visual effect at this viewpoint as mitigation planting will establish during the construction period but will not be sufficiently mature to provide any visual screening or integration of the scheme into views. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES and SES1.	Major adverse (significant)
	Construction night-time	The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of night-time lighting associated with the Crewe North RSD satellite compounds 1 and 2, A530 Nantwich Road satellite compound and MA02 Borrow Pit A satellite compound introducing additional light sources in the near distance and far distance skyglow. During construction the AP1 amendment will not change the night-time visual effect at this viewpoint as mitigation planting will establish during the construction period but will not be sufficiently mature to provide any visual screening. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES and SES1.	Major adverse (significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of the introduction of the large-scale infrastructure across the majority of the view, including Walley's Green embankment, its associated landscape earthworks and mitigation planting, Crewe North RSD and the A530 Nantwich Overbridge. At year 1, the AP1 amendment will slightly change the visual effect at this viewpoint as the landscape earthworks, provided as part of the original scheme and amended by the SES1 design change, enhancement of landscape mitigation at Walley's Green embankment (SES1-002-001) will provide some additional screening, visible in the middle distance and to the north-west for receptors at this location, most notably for residents of Yewtree Farm. However, when considered in the context of the SES1 scheme, the view will remain unchanged for the majority of receptors at this viewpoint. Therefore, the AP1 amendment will give rise to a different significant effect however the level of significance of the effect will remain as reported in the main ES and SES1.	Major adverse (significant)
	Night-time	The main ES and the SES1 reported a moderate adverse (significant) effect. This would be a result of operational lighting associated with the Crewe North RSD introducing lighting elements in the middle-distance of the view, in a predominantly rural and unlit area. At year 1, the AP1 amendment will slightly change the night-time visual effect at this viewpoint as mitigation planting will provide some additional visual screening of lighting. However, light sources will remain visible above the tree line. Therefore, the AP1 amendment will give rise to a different significant effect however the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
Year 15	Summer	The main ES and the SES1 reported a moderate adverse (significant) effect. This would be a result of maturing mitigation planting in association with landscape earthworks which would partially screened Walley's Green embankment, Crewe North RSD and the A530 Nantwich overbridge and would help to integrate the original scheme into views across the wider landscape. However, the SES1 revised scheme, which includes the enhancement of landscape mitigation at Walley's Green embankment (SES1-002-001) would remain visible across the majority of the view, with some elements apparent above the line of mitigation planting. At year 15, the AP1 amendment will slightly change the visual effect at this viewpoint as maturing mitigation planting, provided as part of the original scheme would also be increased in area by the SES1 scheme. This will provide some additional density to the screening in the middle distance and to the north-west for receptors at this location, most notably for residents at Yewtree Farm, further filtering views of the SES1 scheme. However, when considered in the context of the SES1 scheme, the view will remain unchanged for the majority of receptors at this viewpoint. Therefore, the AP1 amendment will give rise to a different significant effect however the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
	Night-time	The main ES and the SES1 reported a moderate adverse (significant) effect. This would be a result of operational lighting at Crewe North RSD, being noticeable in the middle-distance of the view in a predominantly rural and unlit area. However, views were partially screened by maturing landscape mitigation planting. At year 15, the AP1 amendment will slightly change the visual effect at this viewpoint at night-time as maturing landscape mitigation planting, provided as part of the original scheme and amended by increased depth in depth by the AP1 amendment, will partially screen light sources at the Crewe North RSD. However, lighting will remain noticeable in the middle-distance of the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)

Viewpoint 306-02-005: view west from Bellsmithy, A530 Nantwich Road

Visual impact assessment continued

		Permanent effects during operation	Significance of effect
Year 30	Summer	The main ES and SES1 reported a moderate adverse (significant) effect. This would be a result of mature mitigation planting in association with landscape earthworks, further integrating the A530 Nantwich Road into views of wider landscape and providing a denser screen to views of the Crewe North RSD, although the roofline of the maintenance shed remained apparent above the tree canopies. At year 30, the AP1 amendment will slightly change the visual effect at this viewpoint as the further maturing of mitigation planting provided as part of the original scheme, increased in area by the SES1 scheme, which includes the enhancement of landscape mitigation at Walley's Green embankment (SES1-002-001) in the middle distance and to the north-west for receptors at this location, most notably for residents at Yewtree Farm. However, when considered in the context of the SES1 scheme, the view will remain unchanged for the majority of receptors at this viewpoint. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
	Night-time	The main ES and SES1 reported a moderate adverse (significant) effect. This would be a result of operational lighting associated with the Crewe North RSD remaining noticeable in the middle-distance of the view in a predominantly rural and unlit area, with some views partially screened by mature landscape mitigation planting. At year 30, the AP1 amendment will slightly change the visual effect at this viewpoint as mature mitigation planting provided as part of the original scheme and increased in area by the SES1 scheme will further filter views of the SES1 scheme. However, lighting will remain noticeable in the middle-distance of the view above the tree canopy. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

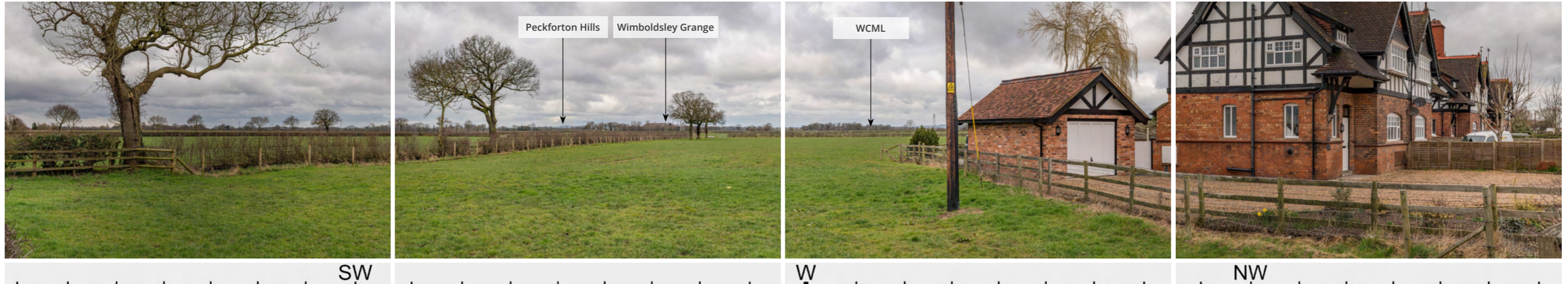
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Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Winter view (baseline)

Date taken: 27/03/2018 Time taken: 11:55



Summer view (baseline)

Date taken: 08/10/2018 Time taken: 09:23



Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens	
Approximate GPS co-ordinates ref.	368921.837, 363619.671	
Elevation:	49.742m AOD	
Value of the viewpoint:	This viewpoint has a medium value, its composition being representative and typical of the open and predominantly agricultural landscape to the south of Middlewich, with the A530 Nantwich Road a noticeable component of the view.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. The viewpoint is also representative of road users and staff and pupils at the school of lower susceptibility, whose attention is less focused on the landscape.	High



Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

Visual baseline description

Winter	In the near distance is a gated field access, set within a continuous roadside managed hedge with infrequent hedge trees. The views from School Cottages and from the public footpath are unrestricted across large arable fields, bordered by managed hedges with intermittently spaced hedge trees and isolated field trees. The wide field of view extends to a middle-distance skyline of hedge trees, copses and woodland belts. Overhead line equipment and train movements on the WCML are evident across much of the middle distance, set against this backdrop. Overhead power lines and pylons punctuate the skyline in the middle distance, along with the roofline of Wimboldsley Grange. The Peckforton Hills form the far-distant horizon. Views west from the adjacent grounds and buildings of Wimboldsley Community Primary School are partially obstructed by the line of the hedge and by School Cottages alongside the A530 Nantwich Road. Views from the rear of properties on Lea Drive are heavily screened by garden and roadside tree planting.
Summer	In summer, the extent of view does not substantially change. The WCML, pylons and gantries remain evident across much of the view. Field boundary hedgerows provide a small degree of additional screening and mature field trees in foliage are more distinctive in terms of size and form.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties. There is far-distance skyglow above Winsford approximately 3km to the north-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
Construction		The main ES and the SES1 reported a major adverse (significant) effect. This would be as result of large-scale construction works including the construction of Crewe North RSD, Walley's Green embankment, its associated landscape earthworks and landscape mitigation planting, and MA02 Borrow Pit A and B to the south and north, into near and middle-distance views. During construction, the AP1 amendment additional land permanently required for the provision of landscape mitigation planting at Wimboldsley (AP1-002-002) will not change the visual effect at this viewpoint as although mitigation planting will be established during the construction period, it will not be sufficiently mature to provide any visual screening or integration of the scheme into views. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES and SES1.	Major adverse (significant)
Construction night-time		The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of night-time lighting associated with the Crewe North RSD satellite compounds 2 and 3, introducing additional light sources into middle-distance views. During construction the AP1 amendment will not change the night-time visual effect at this viewpoint as mitigation planting will establish during the construction period but will not be sufficiently mature to provide any visual screening. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES and SES1.	Major adverse (significant)
Construction cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of overhead line equipment, passing trains, Walley's Green embankment, associated landscape earthworks and mitigation planting and buildings within the Crewe North RSD, being introduced across the majority of the view to the west and creating the skyline for some receptors. At year 1, the AP1 amendment will benefit residents of Wimboldsley and users of Wimboldsley Community Primary School, due to hedgerow planting in addition to advanced hedgerow planting as reported in the main ES, and amended by the enhancement of landscape mitigation at Walley's Green embankment (SES1-002-001), which will assist in partially filtering views in the middle-distance for residents of Wimboldsley and users of Wimboldsley Community Primary School. Wider views of the SES1 scheme will remain more open for residents of School Cottages. Therefore, the AP1 amendment will give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES and SES1.	Major adverse (significant)
	Night-time	The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of lighting associated with the Crewe North RSD and lighting for the stabling sidings being introduced into middle-distance views above establishing mitigation planting propose as part of the original scheme, in a predominantly rural and unlit area. At year 1, the AP1 amendment will not change the night-time visual effect at this viewpoint as maturing hedgerow planting will not change middle-distance views of operational lighting. Wider views of operational lighting will remain more open for residents of School Cottages. Therefore, the AP1 amendment will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES and SES1.	Major adverse (significant)
Year 15	Summer	The main ES and the SES1 reported a moderate adverse (significant) effect. This would be a result of maturing mitigation planting in addition to that reported in the main ES, and amended by the SES1 scheme, which would be establishing, in association with landscape earthworks, further filtering views of overhead line equipment and reinstating the intervening field pattern. Views of tall mast lighting and rooflines of the taller Crewe North RSD buildings would remain above the line of mitigation planting proposed as part of the original scheme. At year 15, the AP1 amendment will slightly change the visual effect at this viewpoint as maturing hedgerow planting, in addition to advanced hedgerow planting as reported in the main ES, and amended by the SES1 scheme, will further filter middle-distance views for residents of Wimboldsley and users of Wimboldsley Community Primary School. However, wider views of the SES1 scheme will remain more open for residents of School Cottages. Therefore, the AP1 amendment will give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
	Night-time	The main ES and the SES1 reported a major adverse (significant) effect. This would be a result of night-time lighting associated with the Crewe North RSD and lighting for the stabling sidings in the middle distance, above establishing mitigation planting. At year 15, the AP1 amendment will slightly change the visual effect at this viewpoint. This is because the greater maturity of mitigation planting, in addition to advanced hedgerow planting as reported in the main ES, and amended by the SES1 scheme, which together with this AP1 amendment will further filter middle-distance views for residents of Wimboldsley and users of Wimboldsley Community Primary School. Views of operational lighting for residents of School Cottages will remain unchanged. Therefore, the AP1 amendment will give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES and SES1.	Major adverse (significant)

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

Visual impact assessment continued

		Permanent effects during operation	Significance of effect
Year 30	Summer	The main ES and the SES1 reported a moderate adverse (significant) effect. This would be a result of maturing landscape mitigation planting, provided as part of the original scheme, partially filtering views of overhead line equipment, and the majority of built structures within the Crewe North RSD, beyond which high mast lighting would remain visible on the skyline. At year 30, the AP1 amendment will slightly change the visual effect at this viewpoint as mature hedgerow planting provided by the original scheme and modified by this AP1 amendment will further filter views of the SES1 scheme for residents of Wimboldsley and users of Wimboldsley Community Primary School. However, wider views of the SES1 scheme will remain more open for residents of School Cottages. Therefore, the AP1 amendment will give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
	Night-time	The main ES and the SES1 reported a moderate adverse (significant) effect. This would be a result of night-time lighting associated with the Crewe North RSD and lighting for the stabling sidings, being noticeable elements within the view. At year 30, the AP1 amendment will slightly change the visual effect at this viewpoint as the greater maturity of mitigation planting, in addition to advanced hedgerow planting as reported in the main ES, and amended by the SES1 scheme will further filter middle-distance views for residents of Wimboldsley and users of Wimboldsley Community Primary School. Views of operational lighting for residents of School Cottages will remain unchanged. Therefore, the AP1 amendment will give rise to a different significant effect; however, the level of significance of the effect will remain as reported in the main ES and SES1.	Moderate adverse (significant)
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Current baseline - winter view

Date taken: 27/03/2018. Time taken: 11:55



Winter verifiable photomontage - construction



Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens		
Approximate GPS co-ordinates ref.	368921.837, 363619.671	Direction of View:	264.95°
Elevation:	49.742m AOD	Height of Camera:	1.629m

The viewpoint has been taken approximately 310m away from the AP1 revised scheme. Viewpoint location shown on Map LV-07-307. For full details of the visual assessment at viewpoint 307-02-002 refer to SES1 and AP1 ES Volume 5, Appendix: LV-001-0MA02 Part 4.

This verifiable photomontage (Type 4 as described in Visual Representation of Development Proposals LI TGN 06/19) provides an illustration of how the AP1 revised scheme may look during the peak construction to help inform the visual impact assessment. The construction methods and siting of construction activities of the AP1 revised scheme may be subject to change in response to consultation and ongoing design. The extent of land required temporarily to construct the AP1 revised scheme will not extend beyond that shown in the photomontage. Changes in the construction of the AP1 revised scheme will not result in any significant adverse change in the environmental effects reported in the assessment.

Each individual image represents a 39.6° horizontal field of view with planar projection. At this scale the images do not lend themselves to direct comparison out in the field. Therefore, for viewing in the field, it is recommended that each image from the panoramic photomontage is printed individually, onto an A3 landscape sheet (image size 390mm x 260mm) to be viewed at a comfortable arms length. For further details on the selection of photomontage locations, verifiable methodology and presentation refer to the Landscape and visual assessment Technical Note - Approach to verifiable photomontages (SMR Volume 5, Appendix: CT-001-00001) of the main ES.

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	Map Name Verifiable Photomontage Peak Construction Phase - Winter Viewpoint 307-02-002	

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Current baseline - winter view

Date taken: 27/03/2018. Time taken: 11:55



Winter verifiable photomontage - operation year 1



The viewpoint has been taken approximately 310m away from the AP1 revised scheme. Viewpoint location shown on Map LV-08-307. For full details of the visual assessment at viewpoint 307-02-002 refer to SES1 and AP1 ES Volume 5, Appendix: LV-001-0MA02 Part 4.

This verifiable photomontage (Type 4 as described in Visual Representation of Development Proposals LI TGN 06/19) provides an illustration of how the AP1 revised scheme may look in 2038 (opening year) to help inform the visual impact assessment. The design of the AP1 revised scheme may be subject to design development in response to consultation. Development of detail design after AP1 submission will not result in any significant adverse change in the environmental effects reported in the assessment. Where new planting is proposed, it has been shown as immature plants which would mature over time to further integrate the AP1 revised scheme into the landscape.

Each individual image represents a 39.6° horizontal field of view with planar projection. At this scale the images do not lend themselves to direct comparison out in the field. Therefore, for viewing in the field, it is recommended that each image from the panoramic photomontage is printed individually, onto an A3 landscape sheet (image size 390mm x 260mm) to be viewed at a comfortable arms length. For further details on the selection of photomontage locations, verifiable methodology and presentation refer to the Landscape and visual assessment Technical Note - Approach to verifiable photomontages (SMR Volume 5, Appendix: CT-001-00001) of the main ES.

Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens		
Approximate GPS co-ordinates ref.	368921.837, 363619.671	Direction of View:	264.95°
Elevation:	49.742m AOD	Height of Camera:	1.629m

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	Map Name Verifiable Photomontage Operation Year 1 (2038) - Winter Viewpoint 307-02-002	
		Doc Number: 2PT24-MWJ-EV-MAP-M000-004146-P01
		Date: Jan 2022

Viewpoint 307-02-002: view west from Wimboldsley, A530 Nantwich Road

This viewpoint is representative of views experienced by residents in Wimboldsley village, staff and pupils at Wimboldsley Community Primary School and road users of the A530 Nantwich Road.

Current baseline - summer view

Date taken: 08/10/2018. Time taken: 09:23



Summer verifiable photomontage - operation year 15



The viewpoint has been taken approximately 310m away from the AP1 revised scheme. Viewpoint location shown on Map LV-08-307. For full details of the visual assessment at viewpoint 307-02-002 refer to SES1 and AP1 ES Volume 5, Appendix: LV-001-0MA02 Part 4.

This verifiable photomontage (Type 4 as described in Visual Representation of Development Proposals LI TGN 06/19) provides an illustration of how the AP1 revised scheme may look in 2053 (15 years after opening) to help inform the visual impact assessment. The design of the AP1 revised scheme may be subject to design development in response to consultation. Development of detail design after AP1 submission will not result in any significant adverse change in the environmental effects reported in the assessment. Where new planting is proposed, it has been shown as semi-mature trees which have put on 15 years of growth to illustrate how the AP1 revised scheme will further integrate into the landscape over time.

Each individual image represents a 39.6° horizontal field of view with planar projection. At this scale the images do not lend themselves to direct comparison out in the field. Therefore, for viewing in the field, it is recommended that each image from the panoramic photomontage is printed individually, onto an A3 landscape sheet (image size 390mm x 260mm) to be viewed at a comfortable arms length. For further details on the selection of photomontage locations, verifiable methodology and presentation refer to the Landscape and visual assessment Technical Note - Approach to verifiable photomontages (SMR Volume 5, Appendix: CT-001-00001).

Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens		
Approximate GPS co-ordinates ref.	368921.837, 363619.671	Direction of View:	264.95°
Elevation:	49.742m AOD	Height of Camera:	1.629m

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	Map Name Verifiable Photomontage Operation Year 15 (2053) - Summer Viewpoint 307-02-002	
Community Area MA02: Wimboldsley to Lostock Gralam		<p>Doc Number: 2PT24-MWJ-EV-MAP-M000-004147-P01</p> <p>Date: Jan 2022</p>

Viewpoint 307-02-008: view east from Clive Green Lane, Clive Green

This viewpoint is representative of views experienced by residents along Clive Green Lane and road users of Clive Green Lane.

Winter view (baseline)

Date taken: 12/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 07/09/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	367845, 365151	
Value of the viewpoint:	This viewpoint has a medium value, its composition. It is an open, gently rolling and predominantly agricultural landscape to the west of Middlewich, where the Shropshire Union Canal (Middlewich Branch) is a perceived component of the view.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.	High



Viewpoint 307-02-008: view east from Clive Green Lane, Clive Green

Visual baseline description

Winter	The near distance view is of an established boundary hedge alongside Clive Green Lane, with gaps where field accesses have been created. Properties located along Clive Green Lane look eastward across the line of the road, either with unrestricted views or views heavily screened by garden boundary vegetation. The middle distance is occupied by broad, medium sized fields rising to a shallow crest, bordered by intermittent hedges with infrequent hedge and field trees. The canal is largely screened by its boundary hedge and mature embankment vegetation alongside Clive Green Lane, although the tops of narrowboats are visible where the canal approaches Park Farm. The farm buildings here are visible on the skyline in the middle distance. The far-distance skyline is formed by a network of field boundaries and mature field trees, which in combination limit the extent of views.
Summer	In summer, views from properties are either screened or partially filtered by intervening garden vegetation, roadside hedgerows and mature field trees in leaf. There is an increased sense of enclosure due to summer hedge growth. Views from the road are screened by roadside hedgerows with occasional gaps allowing for brief visibility across adjacent fields.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties. There is a far distance skyglow from the town of Middlewich approximately 1.5km to the east.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a moderate adverse (significant) effect. This would be a result of the introduction of large-scale construction works across the majority of the view in the near and middle distance, including construction of Clive Green Lane realignment and Shropshire Union Canal offline overbridge. During construction, the AP1 amendment, additional land permanently required for the provision of a shared use cycle and pedestrian path at Clive Green Lane (AP1-002-004), will not change the visual effect at this viewpoint. This is because the AP1 amendment will lie beyond the zone of construction of the original scheme and be partially screened by intervening construction activity. The AP1 amendment will be visible in filtered and framed views from Wharf Cottage. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect as reported in the main ES.	Moderate adverse (significant)
	Construction night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of the introduction of night-time lighting associated with Crewe North RSD, Crewe North RSD satellite compound 3, Shropshire Union Canal north and south satellite compounds, Clive Green Lane satellite compound and construction activity related to the Shropshire Union Canal Viaducts Nos. 1-3, which created new areas of light source in the far distance, in a predominantly unlit area. During construction, the AP1 amendment will not change the visual effect at this viewpoint as no additional lighting will be introduced into the view. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect as reported in the main ES.	Moderate adverse (significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a moderate adverse (significant) effect. This would be a result of the realignment of Clive Green Lane, visible in intermittent and partially filtered views from properties on the western side of the Shropshire Union Canal (Middlewich Branch) and across the majority of near-distance views for residents of Wharf Cottages. At year 1, the AP1 amendment will slightly change the visual effect at this viewpoint as the AP1 amendment will be visible in the middle distance of views from the west. Views for residents of Wharf Cottages will be largely filtered through intervening vegetation. In the context of the scale of original scheme, the AP1 amendment will represent a slight change to the view and will be characteristic of its highway context. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a negligible (non-significant) effect. This would be a result of the introduction of lighting at Crewe North RSD generating a far-distance light source above and through existing intervening field vegetation, in a predominantly rural and unlit area. At year 1, the AP1 amendment will not change the visual effect at this viewpoint as no additional lighting will be introduced into the view. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect as reported in the main ES.	Negligible (non-significant)
Year 15	Summer	The main ES reported a minor adverse (non-significant) effect. This would be a result of maturing mitigation planting on the Clive Green Lane earthworks and along the original scheme, partially filtering views of traffic on the local road and train movements. At year 15, the AP1 amendment will slightly change the visual effect at this viewpoint as the AP1 amendment will be partially screened by the intervening Clive Green Lane realignment and additionally by the maturing intervening mitigation planting which forms part of the original scheme. Views for residents of Wharf Cottages will continue to be largely filtered through intervening vegetation. In the context of the scale of the original scheme, the AP1 amendment will result in a slight change to the view and will be characteristic of its highway context. Therefore, the AP1 amendment will give rise to a different effect however, the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The main ES reported a negligible (non-significant) effect. This would be a result of lighting associated with the Crewe North RSD to the south, generating a far distance light source above and through existing intervening field vegetation, in a predominantly rural and unlit area. At year 15, the AP1 amendment will not change the visual effect at this viewpoint as no additional lighting will be introduced into the view. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect as reported in the main ES.	Negligible (non-significant)
Year 30	Summer	The main ES reported a minor adverse (non-significant) effect. This would be a result of mature mitigation planting further filtering views of the original scheme, although the Clive Green Lane overbridge remained intermittently visible on the skyline. At year 30, the AP1 amendment will slightly change the visual effect at this viewpoint as views will be further filtered through mature intervening mitigation planting which forms part of the original scheme. Therefore, the AP1 amendment will give rise to a different effect however, the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The main ES reported a negligible (non-significant) effect. This would be due to the presence lighting associated with the Crewe North RSD to the south being visible as a far-distance light source above and through existing intervening field vegetation, in a predominantly rural and unlit area. At year 30, the AP1 amendment will not change the visual effect at this viewpoint as no additional lighting will be introduced into the view. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect as reported in the main ES.	Negligible (non-significant)
	Operation cumulative	There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 307-02-010: view east from the Shropshire Union Canal (Middlewich Branch), Park Farm

This viewpoint is representative of views experienced by residents of Park Farm, users of Footpath Winsford 3/1, NCN Route 5 and users of the Shropshire Union Canal (Middlewich Branch) including the recreational canal moorings.

Winter view (baseline)

Date taken: 15/01/2019



Summer view (baseline)

Date taken: 19/09/2019



Camera:	Sony a7RII, FFS, Zeiss Loxia 50mm f/2.0 lens	
Approximate GPS co-ordinates ref.	368340.806, 365388.575	
Value of the viewpoint:	This viewpoint has a medium-high value, where the heritage asset of the canal corridor is the main focus of the view within the open agricultural landscape to the west of Middlewich	Sensitivity of the receptor: High
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Similarly, the attention of recreational users of the canal and towpath is focused on the canal and surrounding landscape.	



Viewpoint 307-02-010: view east from the Shropshire Union Canal (Middlewich Branch), Park Farm

Visual baseline description

Winter	The viewpoint is located on the towpath of the Shropshire Union Canal (Middlewich Branch) adjacent to Park Farm. In the near distance the towpath, canal and narrowboat moorings form the primary focus of view, with the towpath flanked by a steep and continuous grassed bank with a managed hedgerow. Outbuildings at Park Farm are a prominent near-distance and skyline element on the southern canal bank. Beyond the canal, the ground gently rises as a series of fields, bordered by hedgerows and infrequent mature trees. The residential property at Park Farm is situated to the south of the farm complex, with open views to the south and east across medium sized, regular fields bordered by managed hedges with infrequent trees.
Summer	In summer the view along the canal corridor does not substantially change, with the open nature of the canal bank to the south allowing open views across farmland. Views from Park Farm residents are partially filtered at by property boundary hedges.
Night-time	Local roads in the area are unlit, with night-time lighting limited to individual properties. There is a far distance skyglow from the town of Middlewich approximately 1.2km to the east.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

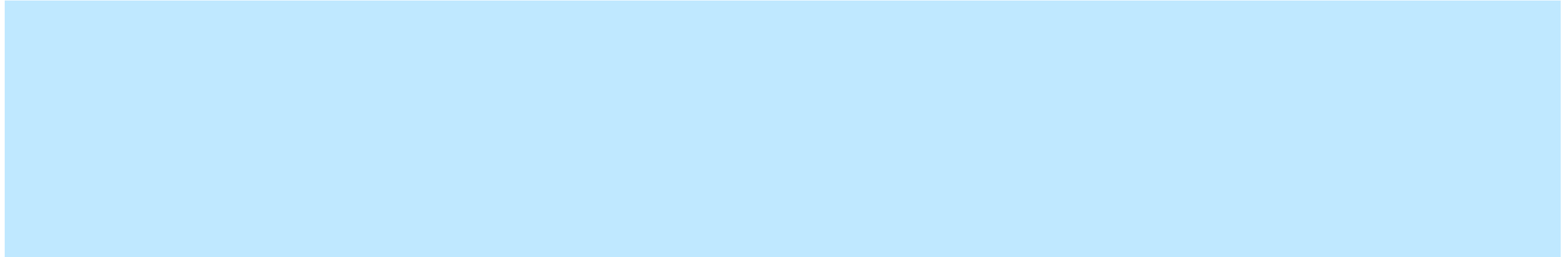
		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a major adverse (significant) effect. This would be a result of the introduction of large-scale construction works into near-distance views and across the majority of the view, including construction of Crewe North RSD reception tracks, Stanthorne south embankment, Clive Green north and south embankments Nos 1-3 and the Shropshire Union Canal Viaducts Nos 1-3. During construction, the AP1 amendment additional land permanently required for the provision of a shared use cycle and pedestrian path at Clive Green Lane (AP1-002-004) will change the visual effect at this viewpoint. This will be due to additional activity in association with construction of a ramp adjacent to the canal that will be introduced into views for the users of the canal and residents of Park Farm. In the context of the scale of the original scheme, the AP1 amendment will represent a minor change to the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Construction night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of the introduction of night-time lighting associated with Shropshire Union Canal north and south satellite compounds, Clive Green Lane satellite compound and the Crewe North RSD in the distance to the south and construction activity in relation to the Shropshire Union Canal Viaducts, which created new areas of light source in the far distance of the view. During construction, the AP1 amendment will not change the visual effect at this viewpoint as the AP1 amendment will not introduce additional lighting into the view. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect reported in the main ES.	Moderate adverse (significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a major adverse (significant) effect. This would be a result of the introduction of infrastructure elements including overhead line equipment and passing trains on Clive Green south and north embankments Nos 1-3 and over the Shropshire Union Canal Viaducts Nos 1-3, into near-distance views, across the width of the view. At year 1, the AP1 amendment will change the visual effect at this viewpoint. This is because the AP1 amendment will introduce a cycleway and new and widened embankments, which will be noticeable in views. The removal of canal side vegetation during construction will open near-distance views towards the cycle path earthworks. In the context of the scale of original scheme, the AP1 amendment will represent a slight change to the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 15	Summer	The main ES reported a major adverse (significant) effect. This would be a result of the original scheme remaining a prominent element spanning the canal corridor despite maturing mitigation planting on Clive Green south and north embankments Nos 1-3, partially filtering views of the lower elements of overhead line equipment and train movements. At year 15, the AP1 amendment will change the visual effect at this viewpoint. This is because the cycle path will remain a noticeable element in views. However, maturing mitigation planting, proposed as part of the AP1 amendment, along the embankments of the cycle/pedestrian path, will partially filter and integrate the AP1 amendment into views along the canal corridor. In the context of the scale of the original scheme, the AP1 amendment will represent a slight change to the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Year 30	Summer	The main ES reported a major adverse (significant) effect. This would be due to the original scheme remaining a prominent element spanning the canal corridor despite the greater maturity of mitigation planting largely filtering views of the lower elements of overhead line equipment and train movements. At year 30, the AP1 amendment will change the visual effect at this viewpoint as the cycleway will remain noticeable in views. However, mature mitigation planting, proposed as part of the AP1 amendment along the embankments of the cycle/pedestrian path, will further filter and integrate the AP1 amendment into views along the canal corridor. In the context of the scale of original scheme, the AP1 amendment will represent a slight change to the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not assessed
Operation cumulative		There are no developments which will result in operational cumulative effects.	No cumulative effect

Viewpoint 308-02-016: view north-west from A54 Chester Road, Middlewich

This new viewpoint is located in an area which would not be affected by the original scheme and is representative of views experienced by residents in properties on the A54 Chester Road, Middlewich and road users.

Winter view (baseline)

It has not been possible to capture winter photography.



Summer view (baseline)

Date taken: 24/09/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	369786 , 366445	
Value of the viewpoint:	This viewpoint has a medium value. The view includes typical features of the urban landscape including the main road lined by mature trees and a mix of residential and commercial properties, street lighting and signage.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	The susceptibility of these receptors is high . Residents have a strong interest in their visual environment. They are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Road users on main roads have a lower susceptibility as their attention is less focused on their surroundings.	High



Viewpoint 308-02-016: view north-west from A54 Chester Road, Middlewich

Visual baseline description

Winter	The A45 Chester Road is prominent in the foreground of the view. A red-brick planter with established ornamental trees and shrubs partially screens the large supermarket on the southern side of the road. Mature trees on both sides of the road frame the view west along the A54 and mature lime trees lining the northern side of the road partially screen the substantial red-brick residential properties and the junction with the A530 Croxton Lane. Hedgerow and brick walls form the boundaries for the residential properties, together with the established street trees, partially screen views towards the A54. The large number of road signs and other street furniture detract from the view.
Summer	In summer, trees in leaf along the A54 Chester Road further filter views of the properties on both sides of the road.
Night-time	The area is lit by street lighting and lighting on residential properties.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	This new viewpoint is located in an area which would not be affected by the original scheme, close to the centre of Middlewich. Residents and road users will experience a noticeable change in views during construction of the AP1 amendment, additional land required for modifications to the A54 Chester Road/A530 Croxton Lane junction (AP1-002-007). The removal of several mature lime trees within the roadside verge will substantially reduce local tree cover and open up views from the residential properties towards the A54 Chester Road and associated traffic movements. Construction activity in the near distance will be prominent in views but not uncharacteristic of the urban setting. There will be a medium magnitude of change and a moderate adverse (significant) effect. The AP1 amendment will therefore give rise to a new significant effect not reported in the main ES.	Moderate adverse (significant)
	Construction night-time	The night-time view in operation was not assessed as there is no requirement for continuous construction lighting in this location.	Not Assessed
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect

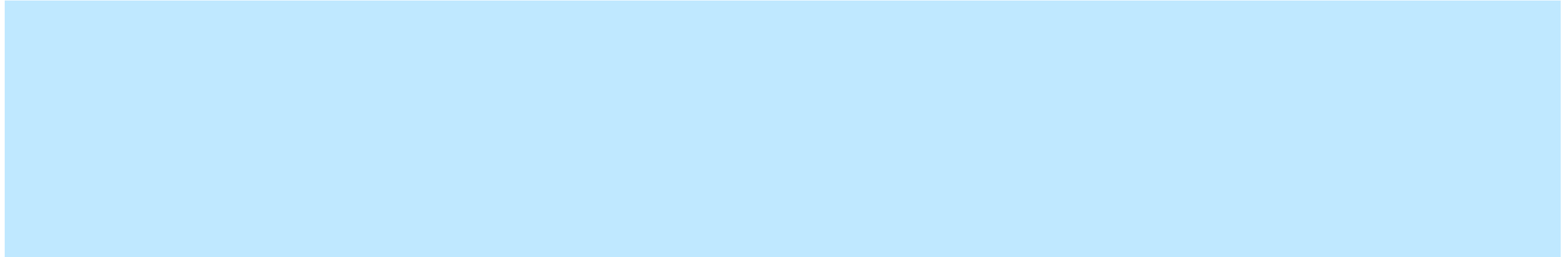
		Permanent effects during operation	Significance of effect
Year 1	Winter	Residents and road users will experience a noticeable change in near distance views as a result of the AP1 amendment. The loss of mature trees in the roadside verge during construction, will reduce local tree cover and open up views from the adjacent residential properties towards the A54 Chester Road and associated traffic movements. Where boundary screening is absent, traffic movements will be perceptible. Where space allows, replacement roadside trees will be planted that will reinforce retained trees in adjacent gardens, but they will be immature in year 1 and will not contribute to the integration of the AP1 revised scheme into the local townscape. There will be a medium magnitude of change and a moderate adverse (significant) effect. The AP1 amendment will therefore give rise to a new significant effect not reported in the main ES.	Moderate adverse (significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 15	Summer	Maturing replacement roadside trees, where verge space permits, in combination with mature trees in adjacent gardens and verges, will partially restore the character of tree-lined views along the highway. There will be a low magnitude of change and a minor adverse (non-significant) effect. The AP1 amendment will therefore give rise to a new non-significant effect not reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 30	Summer	Mature replacement roadside trees, where verge space permits, in combination with mature trees in adjacent gardens, will have largely restored the character of tree-lined views along the highway. There will be a low magnitude of change and a minor adverse (non-significant) effect. The AP1 amendment will therefore give rise to a new non-significant effect not reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Operation cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect

Viewpoint 308-02-018: view east from A54 Middlewich Road and Holly Tree Farm

This new viewpoint is located in an area which would not be affected by the original scheme. This viewpoint is representative of residents in properties including Holly Tree Farm and the Hollies at the junction of the A54 Middlewich Road and Poolford Lane and road users on the A54 Middlewich Road.

Winter view (baseline)

It has not been possible to capture winter photography.



Summer view (baseline)

Date taken: 24/09/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	373966 , 367034	
Value of the viewpoint:	This viewpoint has a medium value. The view includes typical components of the rural landscape, with trees, hedgerows and farmland.	Sensitivity of the receptor: High
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	The susceptibility of these receptors is high . Residents have a strong interest in their visual environment. They are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Road users on main roads have a lower susceptibility as their attention is less focused on their surroundings.	



Viewpoint 308-02-018: view east from A54 Middlewich Road and Holly Tree Farm

Visual baseline description

Winter	The foreground view is of the A54 Middlewich Road bordered by mown grass verges and low, managed hedgerows. A footway runs along the northern side of the road. Gaps in the hedgerows allow glimpsed views of the agricultural landscape beyond the highway boundary. Road signage is visible on the verges and on the central reservation to the east where the eastbound carriageway splits into two lanes at the approach to the motorway junction. Mature, predominantly deciduous, vegetation partially screens vehicles on the raised junction. The M6 is in cutting and therefore not visible. Views east from residential properties are partially screened or filtered by boundary vegetation or garden walls.
Summer	Deciduous vegetation in leaf further filters views of the motorway junction and the farmland bordering the road corridor.
Night-time	The A54 Middlewich Road and motorway junction are not lit. Residential properties are a source of local lighting and skyglow is apparent above Holmes Chapel.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

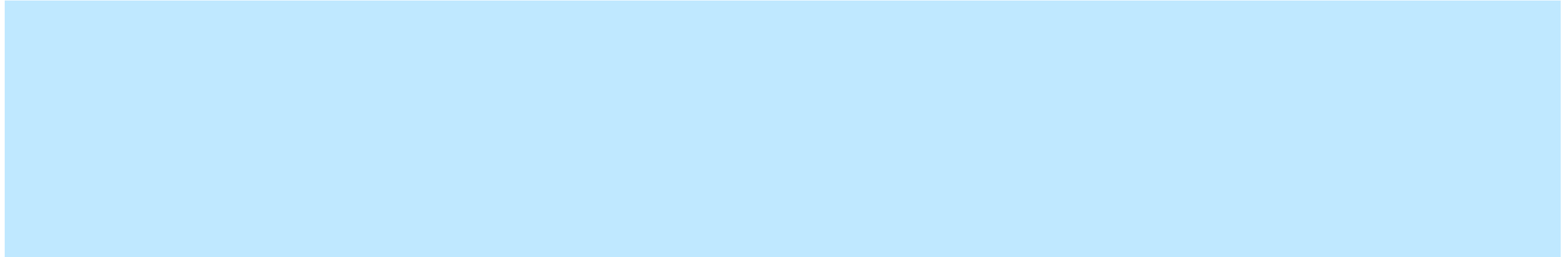
		Temporary effects during construction	Significance of effect
	Construction	This new viewpoint is located in an area which would not be affected by the original scheme. Residents and road users will experience a noticeable change to views to the east. This is a result of construction activity associated with the additional land required for the provision of temporary traffic signals around the M6 junction 18 (AP1-002-008). The AP1 amendment will be visible in the near and middle distance due to the clearance of existing roadside vegetation, including a trimmed hedgerow, semi mature trees and shrubs, which will open up views in the middle-distance to construction plant and machinery. There will be a low magnitude of visual change and a minor adverse (non-significant) effect. The AP1 amendment will therefore give rise to a new non-significant effect not reported in the main ES.	Minor adverse (non-significant)
	Construction night-time	The night-time view in construction was not assessed as there is no requirement for continuous construction lighting in this location.	Not Assessed
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	Residents and users of the A54 Middlewich Road will experience a perceptible change to views a result of vegetation clearance during construction of the AP1 amendment. At year 1, mitigation planting will replace the hedgerow and roadside trees but will not be sufficiently mature to provide any screening or visual integration of the AP1 amendment. There will be a negligible magnitude of change and a negligible (non-significant) effect. The AP1 amendment will therefore give rise to a new non-significant effect not reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 15	Summer	Maturing roadside hedgerows and trees will have largely replaced vegetation lost during construction of the AP1 amendment. Residents and users of the A54 Middlewich Road will experience views that are similar to baseline views. There will be a negligible magnitude of change and a negligible (non-significant) effect. The AP1 amendment will therefore give rise to a new non-significant effect not reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 30	Summer	Mature roadside hedgerows and trees will have replaced the vegetation lost during construction of the AP1 amendment. Residents and users of the A54 Middlewich Road will experience views that are very similar to baseline views. There will be a negligible magnitude of change and a negligible (non-significant) effect. The AP1 amendment will therefore give rise to a new non-significant effect not reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
	Operation cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect

Viewpoint 308-02-019: view west from B5308 Middlewich Road

This new viewpoint is located in an area which would not be affected by the original scheme and is representative of views experienced by residents in properties overlooking the B5308 Middlewich Road and road users.

Winter view (baseline)

It has not been possible to capture winter photography.



Summer view (baseline)

Date taken: 4/09/2021 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	375472 , 367269	
Value of the viewpoint:	This viewpoint has a medium value as the view includes typical components of the road corridor and the suburban landscape on either side of the road, as well as mature trees and hedges which contribute to the green setting of the view.	Sensitivity of the receptor:
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	The susceptibility of these receptors is high . Residents have a strong interest in their visual environment. They are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Road users on main roads have a lower susceptibility as their attention is less focused on their surroundings.	High



Viewpoint 308-02-019: view west from B5308 Middlewich Road

Visual baseline description

Winter	The foreground view is of the B5308 Middlewich Road, bordered by grass verges with street lighting, road signage and mature deciduous trees. The trees are a prominent feature of the view, providing vertical emphasis and visual enclosure of the road corridor, separating it from the adjacent built development. Residential properties on both sides of the road are partially screened by the street trees, well-managed hedges on property boundaries and vegetation growing in gardens. The junction with the A54 Chester Road is visible in the middle distance. Views from residential properties are partially filtered by garden boundary vegetation together with the established street trees.
Summer	In summer leaf cover associated with the mature trees in the roadside verge and the hedgerows further filter views of the properties beyond the highway boundary and enhance the sense of visual enclosure.
Night-time	The area is lit by street lighting and lighting on residential properties.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	This viewpoint is located in an area which would not be affected by the original scheme. Residents and road users will experience a noticeable change to near and middle-distance views as a result of vegetation clearance and construction activity in association with the AP1 amendment, additional land permanently required for modifications to the A54 Middlewich Road and Chester Road junction (AP1-002-009), including changes to the A54 Middlewich Road and Chester Road junction and the realignment of the local road. The removal of several mature roadside trees, in the context of those retained beyond the CCB, will be noticeable. For residents, construction activity will be partially screened by boundary vegetation, particularly in summer months, however road users will experience direct, uninterrupted views of construction activity. There will be a low magnitude of visual change and a moderate adverse (significant) effect. The AP1 amendment will give therefore give rise to a new significant effect not reported in the main ES.	Moderate adverse (significant)
	Construction night-time	The night-time view in construction was not assessed as there is no requirement for continuous construction lighting in this location.	Not Assessed
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	Residents and road users will experience a noticeable change to views a result of the loss of several mature roadside trees during construction, impacting the tree-lined character of the immediate view. For residents, changes to the junction layout will be partially screened by garden boundary vegetation, particularly in summer months. However, road users will experience direct views of the amended highway layout associated with the AP1 amendment. At year 1, replacement trees will be immature and will not contribute towards restoring views along a tree lined highway. There will be a low magnitude of visual change and a moderate adverse (significant) effect. The AP1 amendment will give therefore give rise to a new significant effect not reported in the main ES.	Moderate adverse (significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 15	Summer	Maturing roadside trees will partially restore the character of views along the tree-lined highway. Intervening garden vegetation will partially screen views of the AP1 amendment for residents. However, road users will continue to experience direct views of the amended junction layout. There will be a negligible magnitude of visual change and a negligible (non-significant) effect. The AP1 amendment will give therefore give rise to a new non-significant effect not reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 30	Summer	Mature roadside trees will largely restore views along the tree lined highway. Intervening garden vegetation will continue to screen views of the AP1 amendment for of residents. However, road users will still experience direct views of the amended junction layout. There will be a negligible magnitude of visual change and a negligible (non-significant) effect. The AP1 amendment will give therefore give rise to a new non-significant effect not reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation was not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Operation cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect

Viewpoint 312-02-001: view north-east from Springbank Farm, Birches Lane

This viewpoint is representative of views experienced by residents of Springbank Farm and residential properties on Birches Lane.

Winter view (baseline)

Date taken: 07/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 21/08/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length
Approximate GPS co-ordinates ref.	369211, 373977
Value of the viewpoint:	This viewpoint has a medium value, its composition being of a transition between the townscape of Northwich and open farmland to the east of the town. Traffic on the A556 Shurlach Road is a noticeable component of the view.
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme.

Sensitivity of the receptor:
High



Viewpoint 312-02-001: view north-east from Springbank Farm, Birches Lane

Visual baseline description

Winter	The viewpoint is located on Birches Lane, alongside Springbank Farm to the west of the A556 Shurlach Road. The near distance looks across Birches Lane and over a grassed embankment with gappy hedges, towards large arable fields. Overhead power lines on timber poles run alongside the road and across the fields. A copse of trees surrounding a nearby pond is prominent along Birches Lane to the south-east, interrupting an otherwise far-reaching view across gently undulating farmland. The middle distance comprises the A556 Shurlach Road corridor to the east, heavily filtered by mature vegetation which forms a largely continuous tree canopy beyond nearby fields and along the skyline. Riparian vegetation is visible across the line of view, where fields descend towards Wade Brook. The roofline of a residential property and an electricity substation to the far side of Wade Brook are visible in the middle-distance. The far-distance skyline is formed of residential development bordering the Mid-Cheshire Line railway at Lostock Gralam, comprising the roof lines of properties and mature tree canopies.
Summer	The open nature of the view does not substantially change during summer months. The foliage of the copse surrounding the nearby pond is more prominent within the view, while vegetation bordering the A556 Shurlach Road forms a dense screen to traffic movements along the road corridor. The summer foliage of riparian vegetation bordering Wade Brook filters views towards the substation, while the skyline of Lostock Gralam remains uninterrupted.
Night-time	Street lighting along Birches Lane is evident in the near distance of view, while the A556 Shurlach Road and residential edge of Lostock Gralam contribute to far-distance skyglow. Other light sources are limited to individual properties.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
Construction		The main ES reported a major adverse (significant) effect. This would be as a result of the introduction of large-scale construction activity including Birches Lane satellite compound, the realignment of the A556 Shurlach Road and construction of Rudheath embankment into near and middle-distance views. During construction, the AP1 amendment, additional land permanently required for the underground diversion of a 11kV Scottish Power Energy Network overhead line at Birches Lane (AP1-002-011) will slightly change the visual effect at this viewpoint. This is because, the nature and scale of the rerouting of the cable diversion from the north of the properties (original scheme) to the south of the properties (the AP1 amendment) will allow for retention of garden vegetation at a couple of residential properties on Birches Lane, resulting in a slight improvement in the view, compared with the original scheme. However, the AP1 amendment will be a minor and highly localised change in the context of the large-scale of construction works for the original scheme, which will be prominent in the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance of the effect will remain as reported in the main ES.	Major adverse (significant)
Construction night-time		The main ES reported a moderate adverse (significant) effect. This would be as a result of the presence of night-time lighting for the Birches Lane satellite compound, Lostock Gralam Viaduct satellite compound and construction activity for the Wade Brook and Lostock Gralam Viaducts in the near, middle and far distance of the view. During construction, the AP1 amendment will not change the visual effect at this viewpoint as no additional lighting will be introduced a result of the AP1 amendment. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect as reported in the main ES.	Moderate adverse (significant)
Construction cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a major adverse (significant) effect. This would be a result of the introduction of large-scale infrastructure elements including the realigned A556 Shurlach Road, Rudheath embankment and Wade Brook Viaduct into the near and middle distance of the view. At year 1, the AP1 amendment will slightly change the visual effect at this viewpoint as the route of the electricity cable diversion will have allowed for retention of garden vegetation at a couple of properties on Birches Lane. This will be a noticeable improvement in the view for these receptors compared with the original scheme. However, given the large-scale of the original scheme, including embankments and viaducts, the AP1 revised scheme will still be prominent in the view. Therefore, the AP1 amendment will give rise to a different significant effect however, the level of significance will remain as reported in the main ES.	Major adverse (significant)
	Night-time	The main ES reported a moderate adverse (significant) effect. This would be a result of the introduction of street lighting and traffic headlights, much closer in the view due to the loss of mature roadside vegetation during construction. At year 1, the AP1 amendment will not change the visual effect at this viewpoint as the AP1 amendment will not introduce additional operational lighting into views. Therefore, the AP1 amendment will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
Year 15	Summer	The main ES reported a moderate adverse (significant) effect. This would be a result of maturing landscape mitigation planting alongside the A556 Shurlach Road realignment and route of the original scheme, partially integrating the realigned A556 Shurlach Road and lower elements of train movements and overhead line equipment on Rudheath embankment, into views. At year 15, the AP1 amendment will not change the visual effect at this viewpoint. This is because, the AP1 amendment will not be discernible in the view. Therefore, the AP1 amendment will not give rise to a different significant effect at this viewpoint and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a minor adverse (non-significant) effect. This would be a result of maturing mitigation planting alongside the A556 Shurlach Road realignment and route of the original scheme filtering views of headlights. At year 15, the AP1 amendment will not change the visual effect at this viewpoint. This is because the AP1 amendment will not introduce additional operational lighting. Therefore, the AP1 amendment will not give rise to a different significant effect at this viewpoint and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
Year 30	Summer	The main ES reported a moderate adverse (significant) effect. This would be a result of mature landscape mitigation planting further integrating the embankment and viaduct into views and providing a denser screening to train movements and overhead line equipment. However, the realigned A556 Shurlach Road remained evident in views. At year 30, the AP1 amendment will not change the visual effect at this viewpoint. This is because, the AP1 amendment will not be discernible in the view. Therefore, the AP1 amendment will not give rise to a different significant effect at this viewpoint and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Night-time	The main ES reported a minor adverse (non-significant) effect. This would be due to the presence of mature landscape mitigation planting alongside the A556 Shurlach Road realignment and route of the original scheme screening views of headlights. At year 30, the AP1 amendment will not change the visual effect at this viewpoint. This is because the AP1 amendment will not introduce additional operational lighting. Therefore, the AP1 amendment will not give rise to a different effect at this viewpoint and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
Operation cumulative		There are no developments which will result in construction cumulative effects.	No cumulative effect

Viewpoint 312-02-009: view east from the A559 Manchester Road, Lostock Gralam

This viewpoint is representative of views experienced by residents of properties bordering the A559 Manchester Road and Wilson Crescent and by road users.

Winter view (baseline)

Date taken: 07/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 21/08/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length
Approximate GPS co-ordinates ref.	369921, 375356
Value of the viewpoint:	This viewpoint has a medium - low value, its composition being of urban development bordering between Lostock Gralam with open farmland and mature woodland beyond. Traffic movement along the A556 Chester/Shurlach Road and A559 Manchester Road is a prominent component within the view.
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.
Sensitivity of the receptor:	Medium



Viewpoint 312-02-009: view east from the A559 Manchester Road, Lostock Gralam

Visual baseline description

Winter	The near distance comprises the A559 Manchester Road at the junction with the A556 Chester/Shurlach Road in the middle-distance and frequent traffic movement across the line of view. The road is bordered along its northern edge by a high hedge, with residents of properties on Wilson Crescent looking south across the road itself or east across the adjacent arable field. A large at-grade roundabout occupies the middle-distance with wide, grassed verges that allow for open views along the A556 Chester Road corridor. Lighting columns flank the road corridor and break an otherwise wooded far distance skyline comprising Winnington Wood and Long Wood. Lostock Lodge Care Home and Travelodge Northwich occupy the middle-distance to the south-east, bordering the A556 Chester/Shurlach Road. These represent the easternmost development limit of Lostock Gralam.
Summer	The view does not substantially alter during summer months, although the summer foliage of ornamental roadside trees bordering entrance area to Cheshire Business Park effectively screen the A556 Chester/Shurlach Road and the extent of view towards Long Wood.
Night-time	The residential area of Lostock Gralam, the A559 Manchester Road and the A556 Chester/Shurlach Road all comprise street lighting in the near distance and far distance and form part of the wider skyglow from the town of Lostock Gralam. Beyond the A559 Manchester Road to the north there are no light sources visible.

Future baseline description

Construction (2025)	There is a single committed development that will modify the future baseline in the form of a Costa Coffee drive-through (MA02/304) that will extend marginally the built form to the east and limit the breadth of the view to the south and south-east.
Operation (2038)	There is a single committed development that will modify the future baseline in the form of a Costa Coffee drive-through (MA02/304) that will extend marginally the built form to the east and limit the breadth of the view to the south and south-east.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a moderate adverse (significant) effect. This would be a result of the introduction of large-scale construction activity associated with Lostock Gralam embankment and Smoker Brook Viaduct, into near and middle-distance views. During construction, the AP1 amendment, additional land permanently required for the realignment and extension of Smoker Brook viaduct at the A556 Shurlach Road and Winnington Wood (AP1-002-012), will not change visual effects at this viewpoint. This is because, there will be a barely perceptible change in construction activity in the middle and background of the view. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect as reported in the main ES.	Moderate adverse (significant)
	Construction night-time	The main ES reported a negligible (non-significant) effect. This would be a result of the introduction of night-time lighting for the Lostock Gralam Viaduct satellite compound, Smoker Brook Viaduct south satellite compound and construction activity in relation to the Smoker Brook Viaduct, in a relatively well-lit area. The AP1 amendment will not change the night-time visual effects at this viewpoint as construction lighting will be comparable to the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect as reported in the main ES.	Negligible (non-significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a moderate adverse (significant) effect. This would be a result of the presence of the Smoker Brook Viaduct, visible over the A556 Chester Road and the A559 Manchester Road. At year 1, there will be a slight change to the original scheme resulting in a barely perceptible change in the middle and background of the view. The AP1 amendment, seen in the context of the large-scale structures of the original scheme, will not change the visual effect. Therefore, the AP1 amendment will not give rise to a different significant effect and will not change the level of significance of the effect as reported in the main ES.	Moderate adverse (significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 15	Summer	The main ES reported a minor adverse (non-significant) effect. This would be a result of maturing mitigation planting on the approach to Smoker Brook Viaduct filtering views of train movements. At year 15, the AP1 amendment will not change the visual effect at this viewpoint as the AP1 amendment will be barely perceptible in the context of the large-scale structures and maturing mitigation planting of the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 30	Summer	The main ES reported a minor adverse (non-significant) effect. This would be a result of mature mitigation planting on the approach to Smoker Brook Viaduct further filtering views of train movements and overhead line equipment. At year 30, the AP1 amendment will not change the visual effect at this viewpoint as the AP1 amendment will be barely perceptible in the context of the large-scale structures and mature mitigation planting of the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and will not change the level of significance of the effect as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
	Operation cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect

Viewpoint 313-02-002: view west from the A556 Chester Road, Cranage Villas

This viewpoint is representative of views experienced by residents of properties adjoining the A556 Chester Road at Cranage Villas and along Ascol Drive and users of the A556 Chester Road.

Winter view (baseline)

Date taken: 07/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 21/08/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	370304, 375523	
Value of the viewpoint:	This viewpoint has a medium value, composed of mature woodland belts that surround the A556 Chester Road and which is a prominent component within the view.	Sensitivity of the receptor: High
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Road users have a lower susceptibility as their attention is less focused on the landscape.	



Viewpoint 313-02-002: view west from the A556 Chester Road, Cranage Villas

Visual baseline description

Winter	The viewpoint is situated on the A556 Chester Road at Cranage Villas, at the eastern limit of Lostock Gralam. The near distance comprises the A556 Chester Road with frequent passing traffic, street lighting columns and road signage. Residents of adjacent properties are predominantly screened at ground floor level by mature garden boundary hedges and ornamental tree planting, although views from upper floors are uninterrupted and across the line of the road. Winnington Wood borders the opposite side the A556 Chester Road, forming a dense margin of mature trees and restricting views north of the road corridor. The middle-distance focus is along the A556 Chester Road and towards the roundabout junction with the A559 Manchester Road. Residents of Cranage Villas have oblique views towards the junction area, although heavily filtered by garden trees and the woodland of Winnington Belt to the west. Properties along Ascol Drive face onto a mature woodland belt, which heavily filters views south-west towards the road junction, the continuation of the A556 Shurlach Road and the developed fringe of Lostock Gralam.
Summer	In summer, views are further restricted by the foliage of the surrounding deciduous woodland areas of Winnington Wood and Winnington Belt. The view is focussed along the line of the A556 Chester Road towards Lostock Gralam.
Night-time	The adjacent A556 Chester/Shurlach Road, roundabout junction and A559 Manchester Road to the south-west comprise street lighting in the near distance and middle-distance. Ascol Drive itself is also lit however, woodland cover to the rear of properties on Ascol Drive filters views from the rear towards the main roads and associated lighting. There is a far distance skyglow from the nearby town of Lostock Gralam approximately 500m to the south-west.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a moderate adverse (significant) effect. This would be a result of the removal of woodland along the northern margin of the A556 Chester Road corridor and residents of Cranage Villas experiencing oblique and narrow views along the line of the A556 Chester Road towards construction activity of Smoker Brook Viaduct. During construction, the additional land permanently required for the realignment and extension of Smoker Brook viaduct at the A556 Shurlach Road and Winnington Wood (AP1-002-012) will not change the visual effect at this viewpoint as intervening woodland at Winnington Wood will screen construction activity associated with the AP1 amendment, in oblique views to the north-west. Therefore, the AP1 amendment will not give rise to a different significant effect and the level of significance of the effect will remain as reported in the main ES.	Moderate adverse (significant)
	Construction night-time	The main ES reported a minor adverse (non-significant) effect. This would be due to the presence of lighting associated with the Smoker Brook Viaduct south satellite compound, Lostock Gralam Viaduct satellite compound and associated with the construction of Smoker Brook and Lostock Gralam Viaducts in the middle distance and filtered by intervening vegetation. During construction, the AP1 amendment will not change the visual effect at this viewpoint as additional lighting will not be required. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	TBC
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a minor adverse (non-significant) effect. This would be a result of the introduction of the Lostock Gralam north embankment and Smoker Brook Viaduct and associated train movements into views. At year 1, the AP1 amendment will not change the visual effect at this viewpoint as intervening woodland at Winnington Wood will screen the AP1 amendment, in oblique views to the north-west. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 15	Summer	The main ES reported a minor adverse (non-significant) effect. This would be a result of maturing mitigation planting along Lostock Gralam north embankment, further screening and integrating the original scheme into views. At year 15, the AP1 amendment will not change the visual effect at this viewpoint as intervening woodland at Winnington Wood in association with maturing mitigation planting, will continue to screen the AP1 amendment, in oblique views to the north-west. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 30	Summer	The main ES reported a minor adverse (non-significant) effect. This would be a result of the greater maturity of mitigation planting largely integrating the original scheme into views of woodland, with overhead line equipment and train movements visible on the viaduct across a narrow and oblique line of view. At year 30, the AP1 amendment will not change the visual effect at this viewpoint as intervening woodland at Winnington Wood in association with mature mitigation planting, will continue to screen the AP1 amendment, in oblique views to the north-west. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Operation cumulative		There are no developments which will result in operation cumulative effects.	No cumulative effect

Viewpoint 313-02-003: view east from Footpath Wincham 8/1, Linnards Lane

This viewpoint is representative of views experienced by residents of Smoker Brook farm and Home Farm, and users of Footpath Wincham 8/1 and road users along Linnards Lane.

Winter view (baseline)

Date taken: 14/02/2018 (stitched panorama)



Summer view (baseline)

Date taken: 21/08/2018 (stitched panorama)



Camera:	Nikon D3200 SLR 50mm (equivalent) focal length	
Approximate GPS co-ordinates ref.	369861, 376095	
Value of the viewpoint:	This viewpoint has a medium value, composed of open farmland with large intervening belts of mature woodland.	Sensitivity of the receptor: High
Susceptibility of the receptor to the change arising from the AP1 revised scheme:	Residents have a strong interest in their visual environment and are therefore highly susceptible to visual change arising from the construction and operation of the AP1 revised scheme. Similarly, the attention of footpath users is focused on the surrounding landscape. Road users have a lower susceptibility as their attention is less focused on the landscape.	



Viewpoint 313-02-003: view east from Footpath Wincham 8/1, Linnards Lane

Visual baseline description

Winter	The near distance comprises Linnards Lane with occasional passing traffic, field access gates and managed roadside hedgerows with occasional but prominent mature hedgerows trees. Beyond the roadside hedgerow to the south-east the mid-ground comprises managed hedgerows and numerous hedgerow trees through large semi-improved pastoral fields that gently fall away towards the valley of Wincham Brook. Views culminate in the far distance with a dense belt of mixed woodland that follows Wincham Brook, incorporating Winnington Wood and Peas Wood. No distant views are experienced to the south.
Summer	In summer, the extent and content of views does not substantially change and views are framed by boundary hedgerows and mature hedgerow trees through the very gently undulating farmland. Field trees are more distinctive and prominent in the summer and the woodland to the south screens the views beyond. The belt of deciduous woodland in foliage alongside Wincham Brook screen views north and east.
Night-time	The urban and rural sections of the A556 Chester/Shurlach Road and A559 Manchester Road to the east and south in the far distance are lit. The area between the viewpoint and the main roads is predominantly unlit with a skyglow evident above Lostock Gralam, approximately 1km to the south.

Future baseline description

Construction (2025)	There are no committed developments which will change the baseline.
Operation (2038)	There are no committed developments which will change the baseline.

Visual impact assessment

		Temporary effects during construction	Significance of effect
	Construction	The main ES reported a minor adverse (non-significant) effect. This would be a result of the construction of Smoker Brook Viaduct visible in filtered and oblique views in the middle-distance to the south-east. During construction, the additional land permanently required for the realignment and extension of Smoker Brook viaduct at the A556 Shurlach Road and Winnington Wood (AP1-002-012) will not change the visual effect at this viewpoint. This is because the AP1 amendment will be barely perceptible within the view due to distance from the viewpoint and intervening vegetation. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Construction night-time	The main ES reported a minor adverse (non-significant) effect. This would be due to the presence of night-time lighting for the Smoker Brook Viaduct north satellite compound and construction activity in relation to the Smoker Brook Viaduct in the far distance, above and beyond intervening vegetation. During construction, the AP1 amendment will not change the visual effect at this viewpoint as construction lighting associated with the AP1 amendment will be the same as that required in the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Construction cumulative	There are no developments which will result in construction cumulative effects.	No cumulative effect
		Permanent effects during operation	Significance of effect
Year 1	Winter	The main ES reported a minor adverse (non-significant) effect. This would be a result of the presence of Smoker Brook Viaduct between Leonard's Wood and Winnington Wood, together with overhead line equipment and passing trains, visible across a small portion of the view in the middle-distance. At year 1, the AP1 amendment will not change the visual effect at this viewpoint as the AP1 amendment will be barely discernible in the view due to distance from the viewpoint and intervening vegetation. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Minor adverse (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 15	Summer	The main ES reported a negligible (non-significant) effect. This would be due to the presence of maturing mitigation planting along Smoker Brook south of Linnards Lane and up to Winnington Wood. This largely integrated the viaduct into rural views, but overhead line equipment and passing trains on the elevated viaduct above the line of mitigation planting remained visible. At year 15, the AP1 amendment will not change the visual effect at this viewpoint as it will be barely perceptible in the view due to distance from the viewpoint and intervening existing and maturing mitigation planting associated with the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Year 30	Summer	The main ES reported a negligible (non-significant) effect. This would be a result of mature mitigation planting screening Smoker Brook Viaduct, train movements and overhead line equipment. At year 30, the AP1 amendment will not change the visual effect at this viewpoint as the AP1 amendment will be barely perceptible in the view due to distance from the viewpoint and intervening and mature mitigation planting associated with the original scheme. Therefore, the AP1 amendment will not give rise to a different effect and the level of significance of the effect will remain as reported in the main ES.	Negligible (non-significant)
	Night-time	The night-time view in operation is not assessed as there is no requirement for continuous operational lighting in this location.	Not Assessed
Operation cumulative		There are no developments which will result in operation cumulative effects.	No cumulative effect

AP1 revised scheme: non-significantly affected viewpoints: construction phase

Table 5: AP1 revised scheme schedule of non-significantly affected viewpoints, with justification, construction

Viewpoint number	Construction	Construction night-time	Construction cumulative	Justification
308-02-018	Minor adverse	Not assessed	No cumulative effect	The amendment will give rise to a perceptible change to the features of the view, within the context of the existing road and junction with the M6
313-02-003	Minor adverse	Minor adverse	No cumulative effect	The amendment will not give rise to a perceptible change, views of construction activity in the background will be largely obscured by existing vegetation

AP1 revised scheme: non-significantly affected viewpoints: operation phases

Table 6: AP1 revised scheme schedule of non-significantly affected viewpoints, with justification, operation

Viewpoint number	Operation year 1 (2038)		Operation year 15 (2053)		Operation year 30 (2068)		Operation cumulative	Justification
	Winter	Night-time	Summer	Night-time	Summer	Night-time		
308-02-018	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The amendment will not give rise to a perceptible change in the features of the view
313-02-002	Minor adverse	Not assessed	Minor adverse	Not assessed	Minor adverse	Not assessed	No cumulative effect	The amendment will give rise to a perceptible change in the features of the view, however intervening woodland at Winnington Wood will screen the SES1 scheme and amendment
313-02-003	Minor adverse	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect	The amendment will not give rise to a perceptible change in the features of the view, and be barely discernible in the view due to distance from the viewpoint and intervening vegetation

4.3 AP1 revised scheme assessment matrices

AP1 revised scheme landscape assessment matrix

4.3.1 Table 7 below summarises the assessment of significance for the LCA assessed in the Wimboldsley to Lostock Gralam area (MA02). These are ordered from south to north along the route of the AP1 revised scheme. The assessment of significant effects is presented in SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02), Sections 3 and 4.

Table 7: AP1 revised scheme landscape assessment matrix summarising the assessment of significance for all of the LCA affected by the AP1 revised scheme identified in the Wimboldsley to Lostock Gralam area

LCA	Construction	Construction cumulative	Operation year 1 (2038)	Operation year 15 (2053)	Operation year 30 (2068)	Operation cumulative
Cheshire East/Wimboldsley Plain	Minor adverse	No cumulative effect	Negligible	Negligible	Negligible	No cumulative effect
Middlewich	Minor adverse	No cumulative effect	Negligible	Negligible	Negligible	No cumulative effect
Wincham Brook And Holford Lower Wooded Farmland	Moderate adverse	No cumulative effect	Moderate adverse	Minor adverse	Minor adverse	No cumulative effect
Holmes Chapel	Minor adverse	No cumulative effect	Minor adverse	Negligible	Negligible	No cumulative effect
Winsford and Middlewich Fringe Farmland	Major adverse	No cumulative effect	Moderate adverse	Moderate adverse	Moderate adverse	No cumulative effect
Lostock Plain	Moderate adverse	No cumulative effect	Minor adverse	Minor adverse	Negligible	No cumulative effect

AP1 revised scheme visual assessment matrix

4.3.2 Table 8 below summarises the assessment of significance for all the representative viewpoints assessed as part of AP1 revised scheme in the Wimboldsley to Lostock Gralam area (MA02). These are ordered from south to north along the route of the AP1 revised scheme. The assessment of significant effects is presented in SES1 and AP1 ES Volume 2, Community Area report: Wimboldsley to Lostock Gralam (MA02), Sections 3 and 4. The night-time assessment (reported in Part 4 of this document, as appropriate) has only been undertaken for certain receptors with a view of proposed continuous lighting during either construction or operation. Further detail on this is provided within the Technical Note: Approach to night-time assessment, contained within the SMR. In most cases, in urban areas, additional lighting is not considered to give rise to significant effects due to the widespread presence of street lighting, lightspill from adjacent buildings and skyglow. Where there is no direct foreground visibility of additional lighting, no further assessment has been undertaken.

Table 8: AP1 revised scheme visual assessment matrix summarising the assessment of significance for all of the viewpoints affected by the AP1 revised scheme identified in the Wimboldsley to Lostock Gralam area

Viewpoints		Construction			Operation year 1 (2038)		Operation year 15 (2053)		Operation year 30 (2068)		Operation cumulative
		Winter	Night-time	Cumulative	Winter	Night-time	Summer	Night-time	Summer	Night-time	
306-02-005	View west from Bellsmithy, A530 Nantwich Road	Major adverse	Major adverse	No cumulative effect	Major adverse	Moderate adverse	Moderate adverse	Moderate adverse	Moderate adverse	Moderate adverse	No cumulative effect
307-02-002	View west from Wimboldsley, A530 Nantwich Road	Major adverse	Major adverse	No cumulative effect	Major adverse	Major adverse	Moderate adverse	Major adverse	Moderate adverse	Moderate adverse	No cumulative effect
307-02-008	View east from Clive Green Lane, Clive Green	Moderate adverse	Moderate adverse	No cumulative effect	Moderate adverse	Negligible	Minor adverse	Negligible	Minor adverse	Negligible	No cumulative effect
307-02-010	View east from the Shropshire Union Canal (Middlewich Branch), Park Farm	Major adverse	Moderate adverse	No cumulative effect	Major adverse	Not assessed	Major adverse	Not assessed	Major adverse	Not assessed	No cumulative effect
308-02-016	View north-west from A54 Chester Road, Middlewich	Moderate adverse	Not assessed	No cumulative effect	Moderate adverse	Not assessed	Minor adverse	Not assessed	Minor adverse	Not assessed	No cumulative effect
308-02-018	View east from A54 Middlewich Road and Holly Tree Farm	Minor adverse	Not assessed	No cumulative effect	Negligible	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
308-02-019	View west from B5308 Middlewich Road	Moderate adverse	Not assessed	No cumulative effect	Moderate adverse	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect
312-02-001	View north-east from Springbank Farm, Birches Lane	Major adverse	Moderate adverse	No cumulative effect	Major adverse	Moderate adverse	Moderate adverse	Minor adverse	Moderate adverse	Minor adverse	No cumulative effect
312-02-009	View east from A559 Manchester Road, Lostock Gralam	Moderate adverse	Negligible	No cumulative effect	Moderate adverse	Not assessed	Minor adverse	Not assessed	Minor adverse	Not assessed	No cumulative effect
313-02-002	View west from A556 Chester Road, Cranage Villas	Moderate adverse	Minor adverse	No cumulative effect	Minor adverse	Not assessed	Minor adverse	Not assessed	Minor adverse	Not assessed	No cumulative effect
313-02-003	View east from Footpath Wincham 8/1, Linnards Lane	Minor adverse	Minor adverse	No cumulative effect	Minor adverse	Not assessed	Negligible	Not assessed	Negligible	Not assessed	No cumulative effect